



cityofnovi.org

## MARIELLA ESTATES PRO JZ24-43

### **JZ24-43 MARIELLA ESTATES PRO PLAN WITH REZONING 18.750**

Public hearing at the request of Braciolo Brothers, LLC for Planning Commission's recommendation to City Council for a Zoning Map Amendment from Residential Acreage to R-1 One-Family Residential with a Planned Rezoning Overlay. The subject site is approximately 9.4 acres and is located west of Garfield Road, on the north side of Eight Mile Road (Section 31). The applicant is proposing to develop 10 single family lots.

### **REQUIRED ACTION**

Recommendation to City Council on the rezoning request from RA Residential Acreage to R-1 One Family Residential with a Planned Rezoning Overlay.

REVIEW	RESULT	DATE	COMMENTS
Planning	Approval Recommended	7-18-25	<ul style="list-style-type: none"> <li>• <b>Lot width deviation for 2 lots</b> (<i>Supported</i>)</li> <li>• Items to be addressed in Site Plan submittals</li> </ul>
Engineering	Approval Recommended	3-20-25	<ul style="list-style-type: none"> <li>• Items to be addressed in Site Plan submittals</li> </ul>
Landscaping	Approval Recommended	7-3-25	<ul style="list-style-type: none"> <li>• Items to be addressed in Site Plan submittals</li> </ul>
Wetland	Approval Recommended	7-15-25	<ul style="list-style-type: none"> <li>• <b>Wetland Buffer Authorization required</b></li> <li>• <b>Wetland Conservation Easement recommended</b></li> <li>• Items to be addressed in Site Plan submittals</li> </ul>
Woodland			<ul style="list-style-type: none"> <li>• <b>Woodland permit required (a few regulated trees are to be removed)</b></li> <li>• Items to be addressed on subsequent submittals</li> </ul>
Traffic	Approval Recommended	3-20-25	<ul style="list-style-type: none"> <li>• <b>Deviation for below standard centerline radius of private road</b></li> <li>• Items to be addressed in Site Plan submittals</li> </ul>
Fire	Conditional Approval Recommended	11-14-24	<ul style="list-style-type: none"> <li>• Items to be addressed in Site Plan submittals</li> </ul>

## **MOTION SHEET**

### **Approval**

In the matter of JZ24-43 Mariella Estates, with Zoning Map Amendment 18.750, motion to **recommend approval** to City Council to rezone the subject property from RA Residential Acreage to R-1 One-Family Residential with a Planned Rezoning Overlay Concept Plan.

- A. The recommendation includes the following ordinance deviations for consideration by the City Council, *for the reasons noted in italics*:
- 1) Lot Width (Section 3.1.2.D): A Zoning Ordinance deviation to reduce the required lot width for lots 4 and 5 to 98 feet (120 feet required). *The deviation is requested for the two pie-shaped lots near the corner of the road, which otherwise meet the dimensional requirements.*
  - 2) Road Centerline Radius (Sec. 5.10): A Zoning Ordinance deviation for proposed street with 90-foot centerline radius (100-foot radius required).
- B. If the City Council approves the rezoning, the Planning Commission recommends the following conditions be made part of the PRO Agreement:
- 1) The use is limited to a maximum of 10 single family lots, with the lot layout as shown in the PRO Plan.
  - 2) The overall density shall not exceed 1.07 dwelling units per acre.
  - 3) The small wetland area in southeast corner of site shall be preserved. As the applicant indicates in their response letter, a conservation easement will be placed over the wetland and buffer prior to construction. Lot 1 shall include buffer delineation and signage to prevent encroachment/mowing/removal of vegetation.
  - 4) A minimum 25-foot perimeter landscape buffer shall be maintained from the individual lots to the property boundary.
  - 5) The proposed open space (28%) shall be preserved as shown in the PRO Plan as this represents an enhancement beyond what is typically required for an R-1 district.
- C. This motion is made because the proposed R-1 zoning district is a reasonable alternative to the current district and fulfills the intent of the Master Plan for Land Use, and because of the site-specific development features that will result in an overall benefit to the public that outweighs any detrimental impacts of the project:
- 1) The additional homes allowed under the new R-1 District designation will not detract from the project area and, given the anticipated quality of the custom homes, the development generally will be an enhancement to the project area.
  - 2) The amount of open space on the site and the landscaped buffers mirror what has been provided in recent adjacent developments, and exceed what can be required in the R-1 District.
  - 3) The preservation of the wetland area is beneficial to the environment, providing aesthetic, habitat, and stormwater functions.
  - 4) The project as a whole is in the public interest.

- OR -

**Denial**

In the matter of JZ24-43 Mariella Estates, with Zoning Map Amendment 18.750, motion to **recommend denial** to City Council to rezone the subject property from RA Residential Acreage to R-1 One-Family Residential with a Planned Rezoning Overlay Concept Plan...because *[insert any reasons, such as those suggested below]*

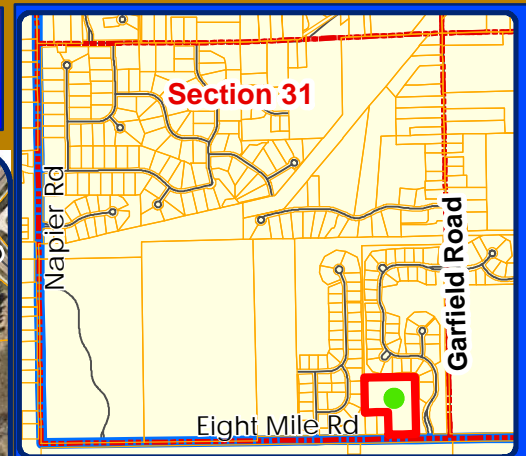
- 1) *The overall benefits of the rezoning do not outweigh the detriments.*
- 2) *The limitations on the ability to develop this property in a way that is related to or consistent with the surrounding residential use is not created by any provision of the zoning ordinance but relates instead to current market conditions.*

MAPS  
Location  
Zoning  
Future Land Use  
Natural Features


---

# JZ24-43 MARIELLA ESTATES

## LOCATION



### Legend

 Subject Area



## City of Novi

Dept. of Community Development  
City Hall / Civic Center  
45175 W Ten Mile Rd  
Novi, MI 48375  
cityofnovi.org

Map Author: Lindsay Bell  
Date: 4/3/25  
Project: MARIELLA ESTATES  
Version #: 1



1 inch = 401 feet

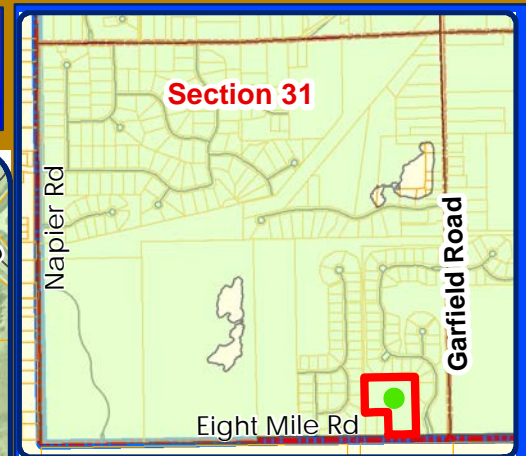
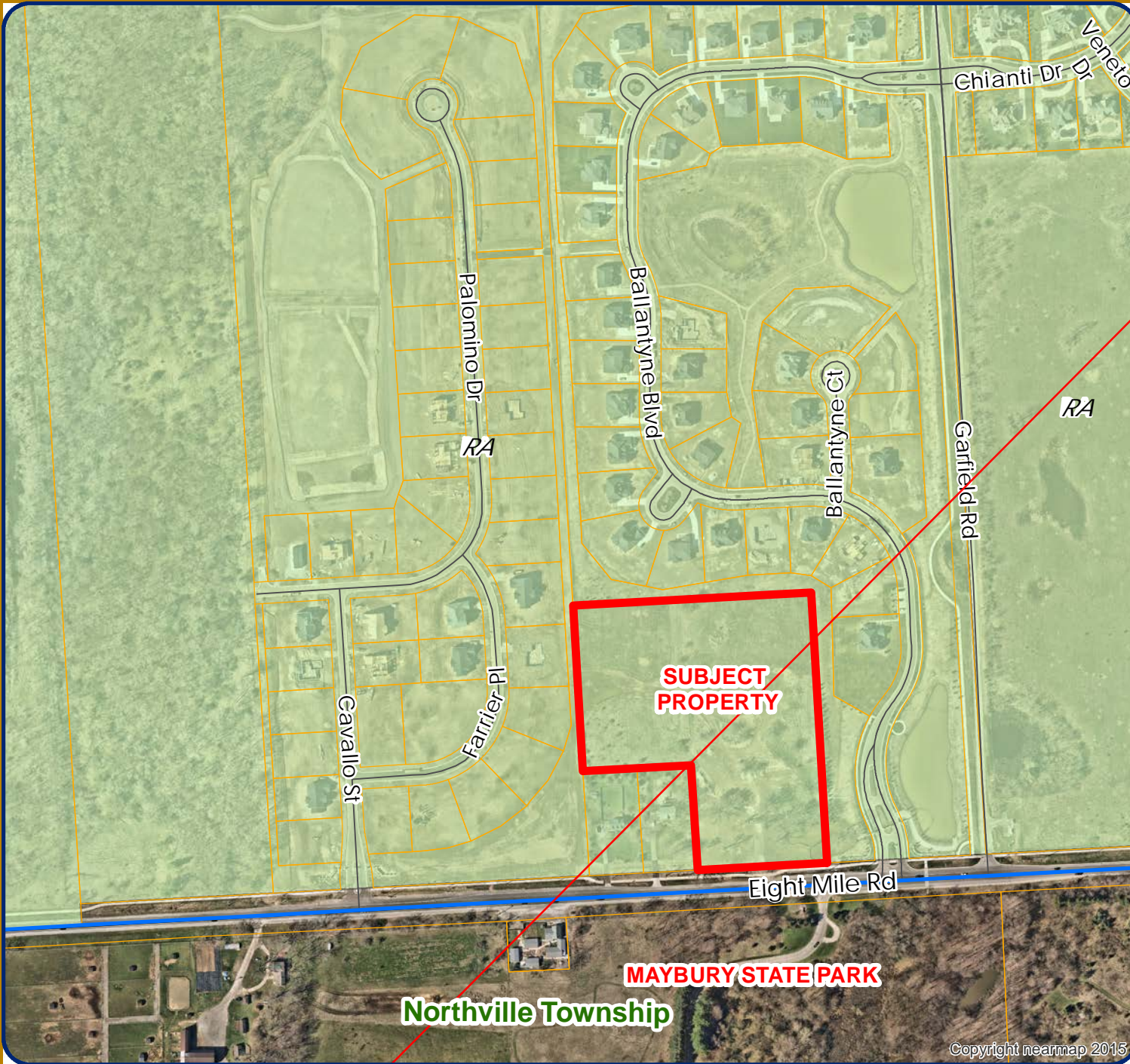


### MAP INTERPRETATION NOTICE


Map information depicted is not intended to replace or substitute for any official or primary source. This map was intended to meet National Map Accuracy Standards and use the most recent, accurate sources available to the people of the City of Novi. Boundary measurements and area calculations are approximate and should not be construed as survey measurements performed by a licensed Michigan Surveyor as defined in Michigan Public Act 132 of 1970 as amended. Please contact the City GIS Manager to confirm source and accuracy information related to this map.


# JZ24-43 MARIELLA ESTATES

## ZONING



### Legend

 R-A: Residential Acreage

 Subject Area



### City of Novi

Dept. of Community Development  
City Hall / Civic Center  
45175 W Ten Mile Rd  
Novi, MI 48375  
cityofnovi.org

Map Author: Lindsay Bell  
Date: 4/3/25  
Project: MARIELLA ESTATES  
Version #: 1



1 inch = 401 feet

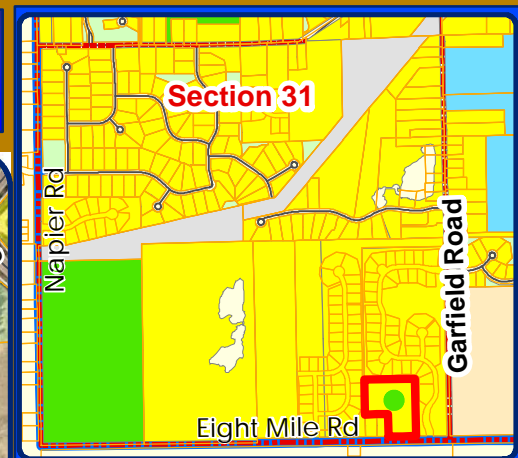
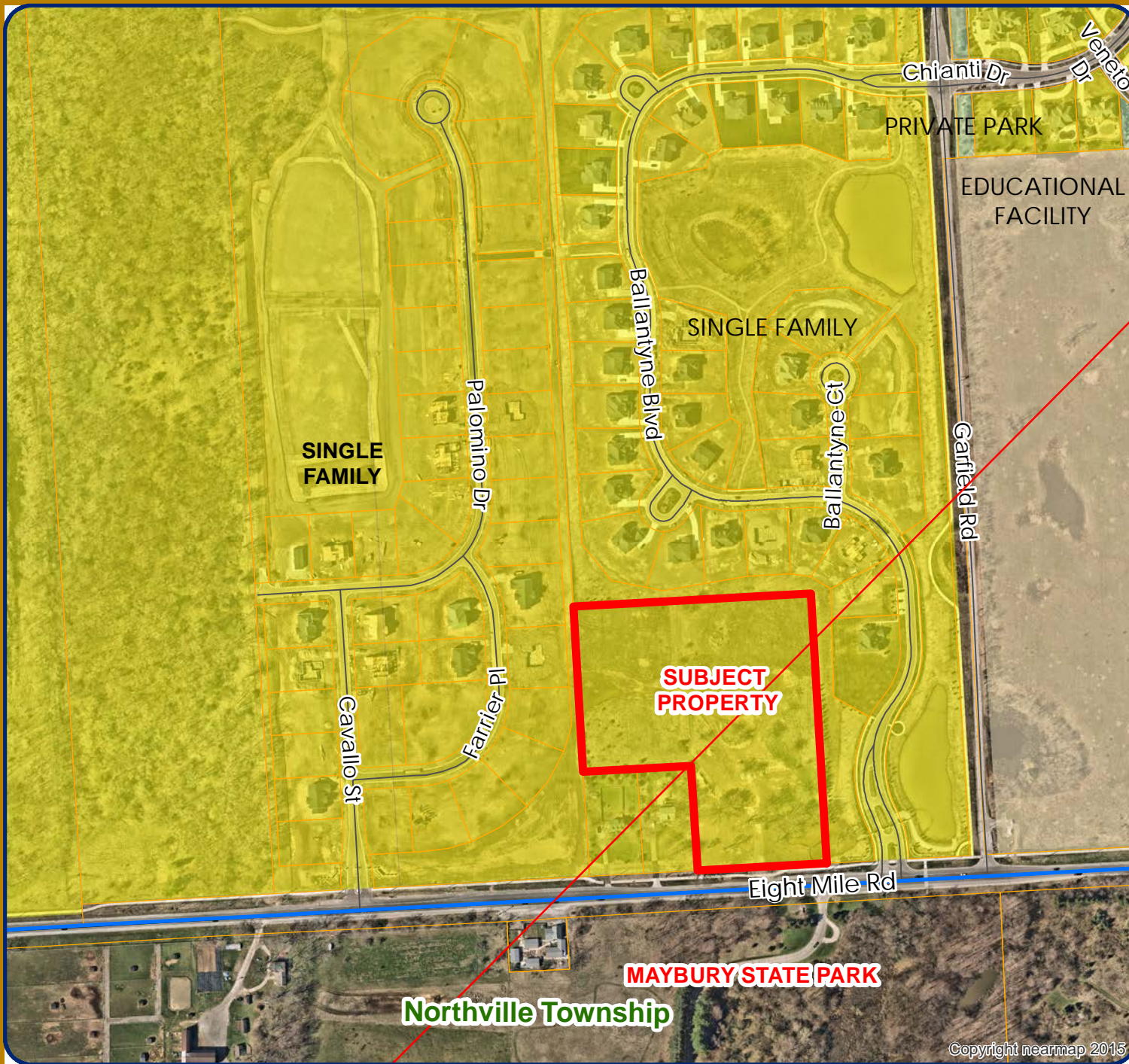


### MAP INTERPRETATION NOTICE

Map information depicted is not intended to replace or substitute for any official or primary source. This map was intended to meet National Map Accuracy Standards and use the most recent, accurate sources available to the people of the City of Novi. Boundary measurements and area calculations are approximate and should not be construed as survey measurements performed by a licensed Michigan Surveyor as defined in Michigan Public Act 132 of 1970 as amended. Please contact the City GIS Manager to confirm source and accuracy information related to this map.

# JZ24-43 MARIELLA ESTATES

FUTURE LAND USE

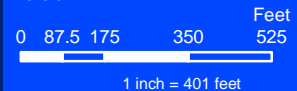


- Legend
- Single Family
  - Educational Facility
  - Public
  - Public Park
  - Private Park
  - Utility
  - Subject Area



City of Novi  
Dept. of Community Development  
City Hall / Civic Center  
45175 W Ten Mile Rd  
Novi, MI 48375  
cityofnovi.org

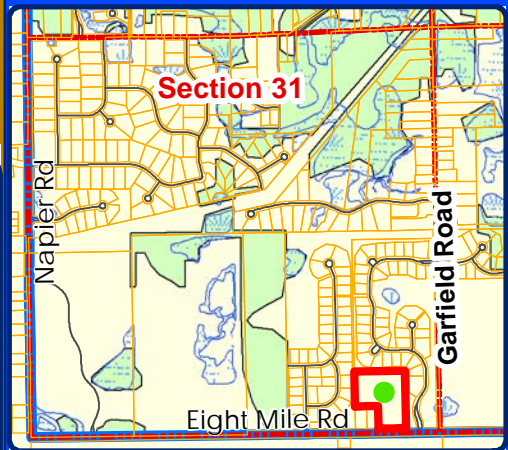
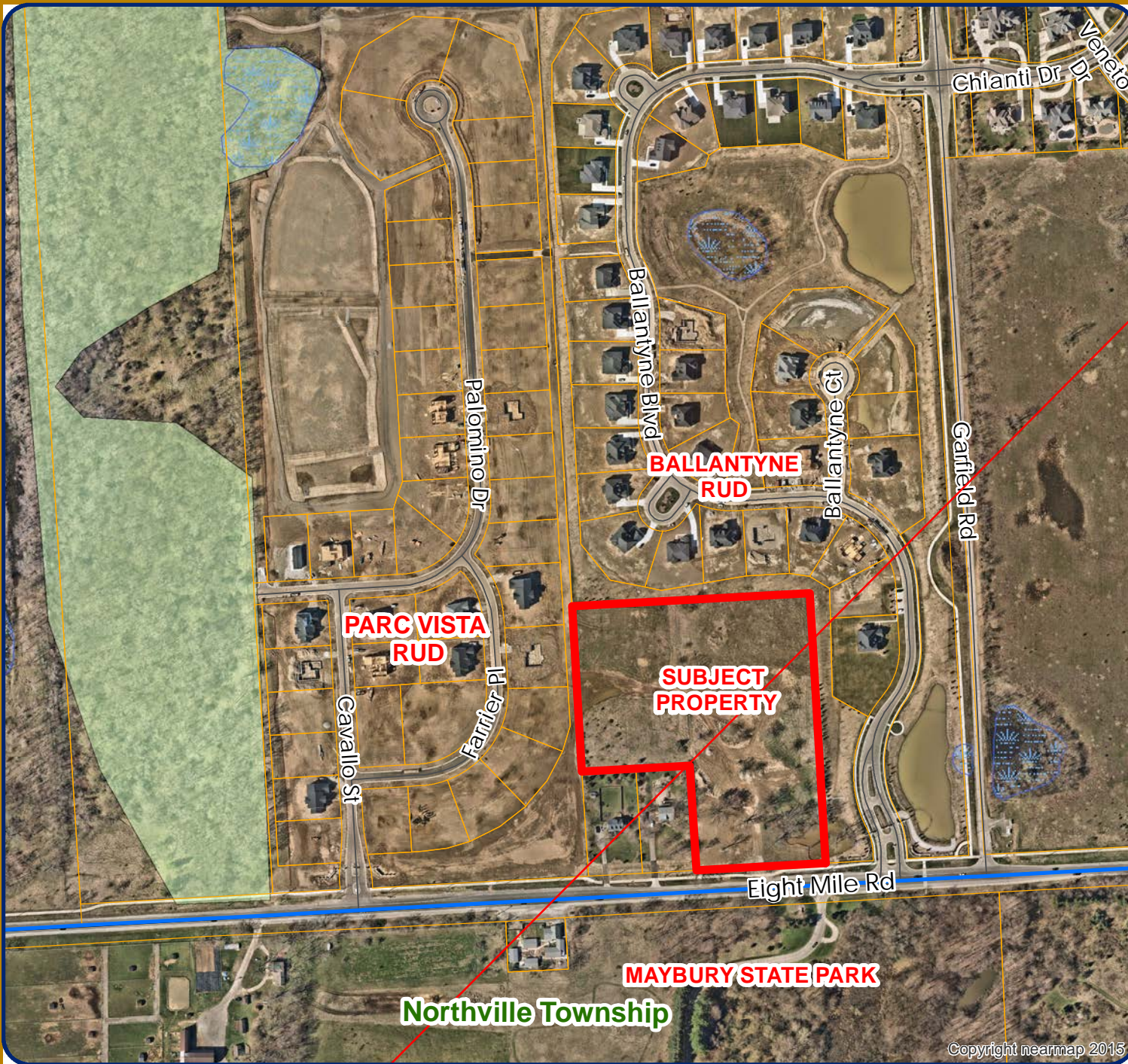
Map Author: Lindsay Bell  
Date: 4/3/25  
Project: MARIELLA ESTATES  
Version #: 1



**MAP INTERPRETATION NOTICE**  
Map information depicted is not intended to replace or substitute for any official or primary source. This map was intended to meet National Map Accuracy Standards and use the most recent, accurate sources available to the people of the City of Novi. Boundary measurements and area calculations are approximate and should not be construed as survey measurements performed by a licensed Michigan Surveyor as defined in Michigan Public Act 132 of 1970 as amended. Please contact the City GIS Manager to confirm source and accuracy information related to this map.

# JZ24-43 MARIELLA ESTATES

## NATURAL FEATURES

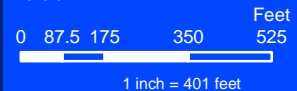


- Legend
- WETLANDS
  - WOODLANDS
  - Subject Area



City of Novi  
Dept. of Community Development  
City Hall / Civic Center  
45175 W Ten Mile Rd  
Novi, MI 48375  
cityofnovi.org

Map Author: Lindsay Bell  
Date: 4/3/25  
Project: MARIELLA ESTATES  
Version #: 1



**MAP INTERPRETATION NOTICE**

Map information depicted is not intended to replace or substitute for any official or primary source. This map was intended to meet National Map Accuracy Standards and use the most recent, accurate sources available to the people of the City of Novi. Boundary measurements and area calculations are approximate and should not be construed as survey measurements performed by a licensed Michigan Surveyor as defined in Michigan Public Act 132 of 1970 as amended. Please contact the City GIS Manager to confirm source and accuracy information related to this map.



# FORMAL PRO PLAN

---

CONCEPTUAL PLANNED REZONING OVERLAY (PRO)

# MARIELLA ESTATES

A SINGLE-FAMILY RESIDENTIAL DEVELOPMENT  
CITY OF NOVI, OAKLAND COUNTY, MICHIGAN

**DEVELOPMENT TEAM**

**DEVELOPER/APPLICANT**  
BRACIOLE BROTHERS, LLC  
47955 ANNA COURT  
WYOMI, MI 48393  
CONTACT: ANTONELLO STANTE  
PHONE: (248) 380-9922

**ENGINEER**  
ATWELL, LLC  
311 NORTH MAIN STREET  
ANN ARBOR, MICHIGAN 48104  
CONTACT: CHRIS ROTHHAAR, P.E.  
PHONE: (947) 886-9874

**SITE DATA**

EXISTING SITE ZONING:	RA, RESIDENTIAL ACREAGE
PROPOSED ZONING:	PRO, PLANNED REZONING OVERLAY (R-1)
PARCELS NO.:	22-31-400-008
EXISTING SITE AREA:	9.36 ACRES
NO. OF SINGLE FAMILY LOTS	10
ANTICIPATED POPULATION	35 (3.5 PER LOT)
MIN. LOT SIZE	21,760 SQ. FT. (0.5 ACRE)
TYP. LOT WIDTH	120' MINIMUM
PROPOSED DENSITY	1.07 DU/ACRE
RA REQUIRED	0.80 DU/ACRE
R-1 REQUIRED	1.65 DU/ACRE
BUILDING SIZE:	35 FEET
HEIGHT:	2
STORIES:	2
FLOOR AREA PER UNIT:	3,200 TO 4,300 SF
OPEN SPACE:	±2.65 AC (28%)

**FIRE DEPARTMENT NOTES**

1. ALL FIRE HYDRANTS AND WATER MAINS SHALL BE INSTALLED AND IN SERVICE PRIOR TO ABOVE FOUNDATION BUILDING CONSTRUCTION AS EACH PHASE IS BUILT.
2. ALL ROADS SHALL BE PAVED AND CAPABLE OF SUPPORTING 35 TONS PRIOR TO CONSTRUCTION ABOVE FOUNDATION.
3. BUILDING ADDRESSES SHALL BE POSTED FACING THE STREET DURING ALL PHASES OF CONSTRUCTION. ADDRESSES SHALL BE A MINIMUM OF THREE INCHES IN HEIGHT ON A CONTRASTING BACKGROUND.
4. FIRE LANES SHALL BE POSTED WITH "FIRE LANE - NO PARKING" SIGNS IN ACCORDANCE WITH ORDINANCE #85.99.02.

**GENERAL NOTES**

1. ALL WORK SHALL CONFORM TO THE CITY OF NOVI'S CURRENT STANDARDS AND SPECIFICATIONS, UNLESS OTHERWISE APPROVED IN THE PRO AGREEMENT.
2. THE CONTRACTOR MUST OBTAIN A PERMIT FROM THE ROAD COMMISSION FOR OAKLAND COUNTY FOR ANY WORK WITHIN THE RIGHT-OF-WAY OF EIGHT MILE ROAD.
3. ALL PAVEMENT MARKINGS, TRAFFIC CONTROL SIGNS, AND PARKING SIGNS SHALL COMPLY WITH THE DESIGN AND PLACEMENT REQUIREMENTS OF THE 2011 MICHIGAN MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
4. THE CITY OF NOVI HAS NO RESPONSIBILITY TO IMPROVE OR MAINTAIN THE PRIVATE STREETS CONTAINED WITHIN OR PRIVATE STREETS PROVIDING ACCESS TO THE PROPERTY DESCRIBED IN THIS PLAN.

**SUMMARY OF PUBLIC BENEFITS**

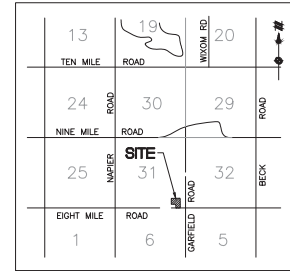
- ITC PARK DONATION: AS PART OF THE DEVELOPMENT THE DEVELOPER PROPOSES A DONATION OF \$40,000 TO BE USED ON IMPROVEMENTS TO THE NEARBY ITC PARK. DETAILS REGARDING SPECIFIC IMPROVEMENTS CAN BE FOUND IN THE LETTER INCLUDED WITH THE SUBMITTAL.
- PERIMETER LANDSCAPE BUFFERS: THE DEVELOPMENT PROPOSES A MINIMUM 25' PERIMETER LANDSCAPE BUFFER FROM THE LOTS TO THE PROPERTY BOUNDARY.
- INCREASED OPEN SPACE: THE DEVELOPMENT PROPOSES AN EXTENSIVE AMOUNT OF OPEN SPACE FOR A SINGLE-FAMILY DEVELOPMENT (28%) AND A MAJORITY OF THE PROPOSED OPEN SPACE IS USABLE ACTIVE OPEN SPACE.
- REDUCED DENSITY: A REDUCED DENSITY OF 1.07 UNITS PER ACRE IS BEING PROPOSED. R-1 ZONING ALLOWS UP TO 1.65 UNITS PER ACRE.

**DEVIATIONS**

- LOTS WIDTHS FOR LOTS 4 AND 5 ARE 98 WIDE. THIS IS 22' BELOW THE 120' MINIMUM LOT WIDTH REQUIRED UNDER R-1 ZONING AS MEASURED AT THE FRONT SETBACK LINE. THESE LOTS STILL PROVIDE ADEQUATE SPACE FOR THE INTENDED HOUSING PRODUCT, ARE THE TWO LARGEST LOTS PROPOSED, AND EXCEED THE R-1 MINIMUM LOT AREA REQUIREMENTS FOR OVER 10,000 SQUARE FEET AND 5,000 SQUARE FEET RESPECTIVELY.
- NO CUL-DE-SAC IS BEING PROVIDED. GIVEN THE LOW VOLUME OF TRAFFIC THAT THIS SUBDIVISION WILL ENCOUNTER A T-TURNAROUND IS BEING PROPOSED DUE TO GEOMETRIC CONSTRAINTS AND A WAY TO REDUCE TOTAL PAVEMENT ON SITE. THE DIMENSIONS OF THE PROPOSED TURNAROUND MEETS CURRENT INTERNATIONAL FIRE CODE (IFC) REQUIREMENTS.
- A REDUCED CENTERLINE RADIUS OF 90 DEGREES IS PROPOSED DUE TO GEOMETRIC SITE CONSTRAINTS. THIS DOES NOT PROVIDE A SAFETY CONCERN GIVEN THE SHORT DISTANCE OF THE ROAD, THE LOW TRAVEL SPEED AND THE MINIMAL TRAFFIC VOLUMES EXPECTED.



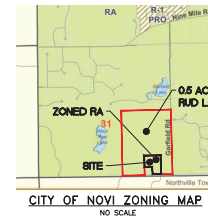
DEVELOPMENT MAP  
SCALE: 1" = 60'



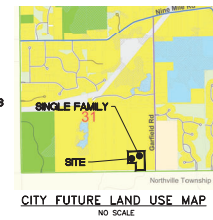
VICINITY MAP  
NO SCALE

**SHEET INDEX**

- 01 COVER SHEET
- 02 EXISTING CONDITIONS
- 03 TREE LIST
- 04 LAYOUT PLAN
- 05 UTILITY PLAN
- 06 GRADING & STORM WATER MANAGEMENT PLAN
- 07 8 MILE FRONTAGE PLAN
- 08 CONCEPTUAL AMENITIES PLAN
- 09 STANDARD DETAILS
- 10 UNDERGROUND DETENTION DETAILS 1
- 11 UNDERGROUND DETENTION DETAILS 2
- 12 UNDERGROUND DETENTION DETAILS 3
- 13 UNDERGROUND DETENTION DETAILS 4
- L-1 LANDSCAPE PLAN
- L-2 GREENBELT & DETENTION
- L-3 LANDSCAPE DETAIL
- L-4 WOODLAND PLAN



CITY OF NOVI ZONING MAP  
NO SCALE



CITY FUTURE LAND USE MAP  
NO SCALE

**PROJECT NARRATIVE**

THE MARIELLA ESTATES PROJECT IS A PROPOSED SINGLE FAMILY RESIDENTIAL COMMUNITY LOCATED ON AN APPROXIMATELY 9-ACRE SITE IN THE CITY OF NOVI, OAKLAND COUNTY, MICHIGAN. THE PROPOSED DEVELOPMENT IS ON 8-MILE ROAD, WEST OF GARFIELD ROAD, AND BETWEEN THE BALANTYNE AND PARC VISTA RUD COMMUNITIES. THE SUBJECT PARCEL IS CURRENTLY ZONED RA AND IS USED AS A RESIDENTIAL PROPERTY. THE DEVELOPMENT WILL UTILIZE THE CITY'S PLANNED REZONING OVERLAY (PRO) OPTION TO ALLOW FOR SINGLE FAMILY HOUSING AT A LOT SIZE AND DENSITY THAT WILL MATCH THE DIRECTLY ADJACENT BALANTYNE AND PARC VISTA DEVELOPMENTS TO THE EAST AND WEST. LANDSCAPE BERMS WILL BE PROVIDED AT THE ENTRANCE TO SCREEN THE DEVELOPMENT FROM 8 MILE ROAD.

THE DEVELOPMENT WILL CONTAIN PRIVATE ROADS AND IS ALSO PROPOSED TO BE SERVED BY PUBLIC SEWER AND WATER LOCATED WITHIN THE 8-MILE ROAD RIGHT-OF-WAY. THESE PUBLIC UTILITIES WILL HAVE THE CAPACITY TO SERVE THE DEVELOPMENT PER THE CITY'S ENGINEERING STANDARDS. STORM WATER MANAGEMENT IS PROPOSED TO BE ADDRESSED THROUGH THE CONSTRUCTION OF AN UNDERGROUND INFILTRATION/RETENTION BASIN ON THE WEST SIDE OF THE PROPERTY. THE POND WILL BE DESIGNED IN ACCORDANCE WITH THE CITY'S REQUIREMENTS.

THE DEVELOPMENT IS PLANNED TO BE CONSTRUCTED IN ONE PHASE.



**ATWELL**  
ANN ARBOR, MI 48106  
481.484.4000

SECTION 31  
TOWN 01 NORTH, RANGE 08 EAST  
CITY OF NOVI  
OAKLAND COUNTY, MICHIGAN

BRACIOLE BROTHERS, LLC  
MARIELLA ESTATES  
CONCEPTUAL PLANNED  
REZONING OVERLAY  
COVER SHEET

DATE: NOVEMBER 6, 2024  
02/28/2025 PER CITY  
06/27/2025 PER CITY

REVISIONS

SCALE: 0 30 60  
1" = 60 FEET

DR. SK CH. CR  
P.L. CR  
BOOK ---  
JOB 23002817  
SHEET NO. 01



**LEGEND**

	BOUNDARY LINE
	EXIST. EASEMENT
	SECTION LINE
	BOUNDARY/PROPERTY LINE
	EXIST. TREE LINE
	EXIST. FENCE
	EXIST. 1' CONTOUR
	EXIST. 5' CONTOUR
	EXIST. WETLAND
	EXIST. WETLAND BUFFER
	EXIST. WATER MAIN
	EXIST. SANITARY SEWER
	EXIST. GAS
	EXIST. STORM SEWER
	EXIST. CULVERT
	EXIST. CATCH BASIN/INLET
	EXIST. HYDRANT
	EXIST. VALVE
	EXIST. SANITARY SEWER

**SITE INFORMATION**

SITE LOCATION: SECTION 31, T.2N., R.5E., CITY OF NOV, OAKLAND COUNTY, MICHIGAN  
ULTIMATE RECEIVING WATER: N/A (ONST)

**EXISTING CONDITIONS NOTES**

1. SOILS INFORMATION REFERENCED FROM USDA NRCS WEB SOILS SURVEY, ACCESSED 2022.
2. SOIL TYPES REFERENCED IN THESE PLANS PER SOIL SURVEY STAFF, NATURAL RESOURCES CONSERVATION SERVICE, UNITED STATES DEPARTMENT OF AGRICULTURE, OAKLAND COUNTY WEB SOIL SURVEY, AVAILABLE ONLINE AT [HTTP://NCDSOILSERVICE.NRCS.USDA.GOV/](http://ncdsoilservice.nrcs.usda.gov/).
3. REFER TO INFILTRATION STUDY PROVIDED BY PEA GROUP, JOB #24-0212, DATED 3/6/2024, TEST PIT LOCATIONS SHOWN IN THESE PLANS PER THE ABOVE REFERENCED REPORT.
4. NO REGULATED FLOODPLAIN EXISTS ON SITE PER FEMA FIRM PANEL F426125C0600F AND F426125C0600F DATED 9/29/2006.
5. NO REGULATED WOODLAND ZONES ARE WITHIN THE PROJECT BOUNDARY. BASED ON INFORMATION GATHERED ONLINE AT [HTTPS://NOV.MAPS-ARJCS.COM/APPS/WEBAPPVIEWER/INDEX.HTM](https://nov.maps-arjcs.com/apps/webappviewer/index.htm).

**SURVEY NOTES**

1. BEARINGS ARE BASED ON MICHIGAN STATE PLANE COORDINATES (NAD83), SOUTH ZONE, GROUND DISTANCES, INTERNATIONAL FEET. VERTICAL DATUM IS BASED ON NAVD83.
  2. THE SITE SHOWN HEREON IS LOCATED WITHIN ZONE 4 (AREAS DETERMINED TO BE OUTSIDE OF THE 0.2% ANNUAL CHANCE FLOODPLAIN) ACCORDING TO MAP NUMBERS 28125C0600F & 28125C0600F OF THE FLOOD INSURANCE RATE MAP, EFFECTIVE DATE SEPTEMBER 29, 2006.
  3. WATER MAIN, STORM SEWER, SANITARY SEWER AND FRANCHISE UTILITY STRUCTURES HAVE BEEN FIELD LOCATED WHERE VISIBLE. UTILITY AND AS-BUILT MAPS HAVE BEEN REQUESTED AND SOME MAPS HAVE BEEN RECEIVED AT DATE OF THIS SURVEY. FRANCHISE UTILITY MAPS HAVE BEEN REQUESTED FROM THE APPROPRIATE FRANCHISE COMPANIES, BUT NOT ALL MAPS HAVE BEEN RECEIVED AT DATE OF SURVEY.
- NOTE: THE SURVEYOR MAKES NO GUARANTEES THAT THE UNDERGROUND UTILITIES SHOWN COMPRISE ALL SUCH UTILITIES IN THE AREA, EITHER IN SERVICE OR ABANDONED.
- NOTE TO THE CLIENT, INSURER, AND LENDER - SOURCE INFORMATION FROM PLANS AND MARKINGS WILL BE COMBINED WITH OBSERVED EXISTING UTILITIES PURSUANT TO SECTION 5.6.V. TO DEVELOP A VIEW OF THE UNDERGROUND UTILITIES. HOWEVER, LACKING EXCAVATION, THE EXACT LOCATION OF UNDERGROUND UTILITIES CANNOT BE ACCURATELY, COMPLETELY, AND RELIABLY DEPICTED. IN ADDITION, IN SOME JURISDICTIONS, 811 OR OTHER SIMILAR UTILITY LOCATE REQUESTS FROM SURVEYORS MAY BE IGNORED OR RESULT IN AN INCOMPLETE RESPONSE, IN WHICH CASE THE SURVEYOR SHALL NOTE ON THE PLAN OR MAP HOW THIS AFFECTED THE SURVEYOR'S ASSESSMENT OF THE LOCATION OF THE UTILITIES. WHERE ADDITIONAL OR MORE DETAILED INFORMATION IS REQUIRED, THE CLIENT IS ADVISED THAT EXCAVATION AND/OR A PRIVATE UTILITY LOCATE REQUEST MAY BE NECESSARY.
4. THIS SURVEY WAS PERFORMED WITHOUT THE BENEFIT OF A CURRENT TITLE COMMITMENT. EASEMENTS MAY EXIST THAT ARE NOT SHOWN HEREON.
  5. SNOW AND ICE CONDITIONS EXISTED IN THE FIELD THAT MAY RESULT IN SOME PHYSICAL FEATURES NOT BEING VISIBLE AND THEREFORE MAY NOT BE LOCATED AS PART OF THIS SURVEY.

**SOILS LEGEND**

Map Unit	Map Unit Name	Depth to	Flood	Pending
180	Ferri sandy loam, 18 plain, 2 to 6 percent slopes	-80 inches	None	None
200	Glywood loam, 2 to 6 percent slopes	-12 to 30"	None	None

**BENCHMARK NOTES**

SOURCE BENCHMARK:  
CITY OF NOV BENCHMARK ID: 3233  
BENCH TIE IN WEST FACE OF POWER POLE 180 FEET NORTH OF CENTERLINE OF 8 MILE ROAD AND PARALLEL ROAD ELEVATION: 974.31 (NAV08)

SITE BENCHMARKS:  
BM #1: ARROW ON FIRE HYDRANT IN NORTH FACE OF WALL OF 8 MILE ROAD AT SOUTHWEST CORNER OF SITE ELEVATION: 977.04 (NAV08)  
BM #2: SET RAILROAD SPIKE IN SOUTH FACE OF UTILITY POLE IN NORTH RIGHT OF WAY OF 8 MILE ROAD ELEVATION: 978.38 (NAV08)

**LEGAL DESCRIPTION**

DESCRIPTION OF TAX PARCEL NUMBER 22-31-400-008 PER WARRANTY DEED RECORDED IN LIBER 57950, PAGE 432, OAKLAND COUNTY RECORDS  
SITUATED IN THE CITY OF NOV, COUNTY OF OAKLAND, STATE OF MICHIGAN, TO WIT:  
A PART OF THE SOUTHEAST 1/4 OF SECTION 31, TOWN 1 NORTH, RANGE 5 EAST, CITY OF NOV, OAKLAND COUNTY, MICHIGAN, MORE PARTICULARLY DESCRIBED AS: COMMENCING AT THE SOUTHWEST CORNER OF SAID SECTION 31; THENCE NORTH 89°45'31" WEST 440.00 FEET ALONG THE SOUTH LINE OF SAID SECTION 31 AND THE CENTERLINE OF EIGHT MILE ROAD; THENCE NORTH 00°07'09" WEST 60.00 FEET TO THE NORTHERLY RIGHT OF WAY LINE OF SAID EIGHT MILE ROAD, AND THE POINT OF BEGINNING; THENCE NORTH 89°45'31" WEST 350.00 FEET ALONG THE NORTHERLY RIGHT OF WAY LINE OF SAID EIGHT MILE ROAD; THENCE NORTH 00°07'09" WEST 290.41 FEET; THENCE NORTH 89°45'31" WEST 300.00 FEET; THENCE NORTH 00°07'09" WEST 458.59 FEET; THENCE SOUTH 89°45'31" EAST 660.00 FEET; THENCE SOUTH 00°07'09" EAST 750.00 FEET TO THE POINT OF BEGINNING.

**ATWELL**  
CONCEPTUAL PLANNED REZONING OVERLAY

SECTION 31  
TOWN 01 NORTH, RANGE 08 EAST  
CITY OF NOV  
OAKLAND COUNTY, MICHIGAN

CLIENT: BRACIOLE BROTHERS, LLC  
MARELLA ESTATES  
CONCEPTUAL PLANNED REZONING OVERLAY  
EXISTING CONDITIONS

DATE: NOVEMBER 6, 2024  
02/28/2025 PER CITY  
06/27/2025 PER CITY

REVISIONS

SCALE: 0 25 50  
1" = 50 FEET

DR. SK. CH. CR.  
P.L. CR.  
BOOK ---  
JOB 23002817  
SHEET NO. 02

NOTICE: THE LOCATION OF EXISTING UNDERGROUND UTILITIES IS NOT GUARANTEED BY THIS SURVEY. THE CONTRACTOR SHALL DETERMINE THE EXACT LOCATION OF ALL UNDERGROUND UTILITIES AND ALL DAMAGES MUST BE REPAIRED BY THE CONTRACTOR. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY AND ALL DAMAGES TO ANY AND ALL UNDERGROUND UTILITIES. WITHOUT THE PROOF WRITING SIGNED BY ATWELL LLC.

C:\Users\jbradley\OneDrive\Documents\2024\23002817\23002817.dwg - 11/06/2024 10:15:38 AM - 100% (ANSI)

Tree Tag #	Data Code	Scientific Name	Common Name	DBH (inches)	Condition	Bats	Comments	Landmark Tree	To Be Removed
1785	ACPL	<i>Acer platanoides</i>	Norway Maple	24	Good	No		Yes	Yes
1786	PIGL	<i>Picea glauca</i>	White Spruce	18	Fair	No	Dead Branches	No	Yes
1787	ACSAN	<i>Acer saccharum</i>	Silver Maple	52	Fair	No	Fused Trunk	Yes	Yes
1788	ACSA	<i>Acer saccharum</i>	Sugar Maple	22	Fair	No	Topped For Utility Line	No	Yes
1789	ACSA	<i>Acer saccharum</i>	Sugar Maple	25	Fair	No	Topped For Utility Line	Yes	No
1790	MASPP	<i>Malus speciosa</i>	Apple/Crabapple	16	Fair	No	Two Trunk (2T): 12; Dead Main Branches	No	Yes
1791	ACSAN	<i>Acer saccharum</i>	Silver Maple	61.5	Good	No	2T: 29; Fused Trunk	Yes	Yes
1792	JUVI	<i>Juniperus virginiana</i>	Eastern Red Cedar	14.5	Fair	No	Dead Branches, Missing Bark	Yes	Yes
1793	MAGR	<i>Magnolia grandiflora</i>	Magnolia	16	Good	No	Three Trunk (3T): 12, 8	No	Yes
1794	ACSAN	<i>Acer saccharum</i>	Silver Maple	33.5	Good	No		Yes	Yes
1795	ACSA	<i>Acer saccharum</i>	Sugar Maple	11	Good	No		No	Yes
1796	ACSAN	<i>Acer saccharum</i>	Silver Maple	20	Good	No		No	Yes
1797	ACSAN	<i>Acer saccharum</i>	Silver Maple	24	Good	No		Yes	Yes
1798	ACSAN	<i>Acer saccharum</i>	Silver Maple	25	Good	No		Yes	Yes
1799	ACSAN	<i>Acer saccharum</i>	Silver Maple	12	Good	No	Seven Trunk (7T): 11, 11, 8.5, 6.5, 6, 3.5	No	Yes
2237	AEGL	<i>Aesculus glabra</i>	Ohio Buckeye	32.5	Fair	No	Trimmed Main Limb	Yes	Yes
2238	MAGR	<i>Magnolia grandiflora</i>	Magnolia	17.5	Fair	No	2T: 14.5; Missing Branches	No	Yes
2246	GLTR	<i>Gleditsia triacanthos</i>	Honey Locust	36	Good	No		Yes	Yes
2247	ACSAN	<i>Acer saccharum</i>	Silver Maple	35	Fair	No	Dead Limbs, Trimmed	Yes	No
2401	ACSAN	<i>Acer saccharum</i>	Silver Maple	27.5	Good	No		Yes	Yes
2402	ACSAN	<i>Acer saccharum</i>	Silver Maple	40.5	Good	No	Fused Trunk	Yes	Yes
2403	MDAL	<i>Morus alba</i>	White Mulberry	16	Good	No	3T: 13, 11	No	Yes
2404	MDAL	<i>Morus alba</i>	White Mulberry	8.5	Good	No		No	Yes
2405	MDAL	<i>Morus alba</i>	White Mulberry	8	Good	No	2T: 7	No	Yes
2406	MDAL	<i>Morus alba</i>	White Mulberry	17	Good	No	2T: 6; Fused Trunk	No	Yes
2407	MDAL	<i>Morus alba</i>	White Mulberry	12	Good	No	Fused Trunk	No	Yes
2408	ACNE	<i>Acer negundo</i>	Boxelder	8	Good	No		No	Yes
2409	MDAL	<i>Morus alba</i>	White Mulberry	22	Good	No		No	Yes
2410	ACNE	<i>Acer negundo</i>	Boxelder	21	Good	No	2T: 10	No	Yes
2411	ACNE	<i>Acer negundo</i>	Boxelder	13	Good	No	Fused Trunk	No	Yes
2412	FRAN	<i>Prunus avium</i>	Sweet Cherry	16.5	Good	No		No	Yes
2413	ACSAN	<i>Acer saccharum</i>	Silver Maple	18	Good	No		No	Yes
2414	MDAL	<i>Morus alba</i>	White Mulberry	8	Good	No		No	Yes
2415	FRPE	<i>Fraxinus pennsylvanica</i>	Green Ash	10.5	Fair	No	Four Trunk (4T): 9.5, 9.5, 6; Dead Main Trunks	No	Yes
2416	ACNE	<i>Acer negundo</i>	Boxelder	12	Good	No		No	Yes
2417	ACSA	<i>Acer saccharum</i>	Sugar Maple	15	Good	No		No	Yes
2418	PIPU	<i>Picea pungens</i>	Blue Spruce	10.5	Good	No		No	Yes
2419	POTR	<i>Populus tremuloides</i>	Quaking Aspen	8	Good	No		No	No
2420	ACNE	<i>Acer negundo</i>	Boxelder	20	Good	No	2T: 16	No	No
2421	MDAL	<i>Morus alba</i>	White Mulberry	15.5	Good	No	2T: 4; Fused Trunk	No	Yes
2422	RDPS	<i>Robinia pseudoacacia</i>	Black Locust	8	Good	No		No	Yes
2423	RDPS	<i>Robinia pseudoacacia</i>	Black Locust	8	Good	No		No	Yes
2424	RDPS	<i>Robinia pseudoacacia</i>	Black Locust	25	Good	No		Yes	Yes
2425	RDPS	<i>Robinia pseudoacacia</i>	Black Locust	9	Good	No		No	Yes

**311**  
Know what's below.  
Call before you dig.  
THE LOCATION OF UTILITIES AND THE DEPTHS OF UTILITIES ARE SHOWN ON THIS PLAN ONLY AND HAVE NOT BEEN INDEPENDENTLY VERIFIED BY THE CONTRACTOR. THE CONTRACTOR SHALL DETERMINE THE EXACT LOCATION OF ALL UTILITIES PRIOR TO ANY CONSTRUCTION WORK. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND ALL DAMAGES MUST BE REPAIRED BY THE CONTRACTOR. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND ALL DAMAGES MUST BE REPAIRED BY THE CONTRACTOR. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND ALL DAMAGES MUST BE REPAIRED BY THE CONTRACTOR.



CLIENT: BRADCOLE BROTHERS, LLC  
MARIELLA ESTATES  
CONCEPTUAL PLANNED  
REZONING OVERLAY  
TREE LIST

SECTION 31  
TOWN 01 NORTH, RANGE 08 EAST  
CITY OF NOVI  
OAKLAND COUNTY, MICHIGAN

DATE: NOVEMBER 6, 2024  
02/28/2025 PER CITY  
06/27/2025 PER CITY

REVISIONS

SCALE: 0' = 1" NO SCALE

DR. SK. DR. CR.  
P.M. CR.  
BOOK  
JOB: 23002817  
SHEET NO. 08

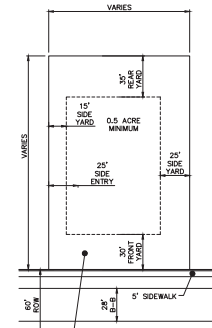


### LAYOUT NOTES

1. THE DEVELOPMENT WILL CONTAIN PRIVATE ROADS. THE PROPOSED CONNECTION WILL BE COORDINATED WITH THE ROAD COMMISSION OF OAKLAND COUNTY.
2. THE ROAD WILL NOT BE GATED.
3. LANDSCAPE BUFFERS WILL BE PROVIDED ON THE SIDES OF THE DEVELOPMENT AS SHOWN BUFFERING THE PROPERTY FROM THE ADJACENT USES.
4. LANDSCAPING SHOWN IS PRELIMINARY AND SUBJECT TO CHANGE.
5. ALL SIDEWALKS AND SIDEWALK RAMPS SHALL BE ADA COMPLIANT AND SHALL COMPLY WITH BARRIER FREE DESIGN STANDARDS.
6. NO ON-STREET PARKING IS PROPOSED. UNIT GARAGE AND DRIVEWAY PARKING EXCEEDS CITY PARKING REQUIREMENTS. NO PARKING SIGNAGE TO BE PLACED IN AREAS OF ROAD CURVE RADIUS LESS THAN 230'.
7. ALL ON-SITE SIGNAGE AND PAVEMENT MARKINGS SHALL BE IN COMPLIANCE WITH THE LIMITEDS.
8. PROPOSED BUILDING FACADES WILL HAVE A MINIMUM OF 30R BROOK OR STONE.
9. THE TOTAL PROPOSED FLOOR SQUARE FOOTAGE OF EACH DWELLING UNIT VARIES FROM 3,200 TO 4,300 SQUARE FEET.
10. SIGHT TRIANGLES SHOWN FOR 1.5' EYE HEIGHT IN ACCORDANCE WITH MOST TRAFFIC AND SAFETY STANDARDS APRIL 22, 2015 (2011 ASHTO TABLE 4-6, 4-8, 4-9-38). SIGHT DISTANCE FOR SPEED LIMIT FOR 55 MPH AND 2 LINES OF TRAFFIC IS 610'.
11. CITY OF NOV HAS NO RESPONSIBILITY TO IMPROVE OR MAINTAIN THE PRIVATE STREETS CONTAINED WITHIN OR PRIVATE STREETS PROVIDING ACCESS TO THE PROPERTY DESCRIBED IN THIS PLAN.

### WETLAND SUMMARY

TOTAL WETLAND AREA: 0.11 ACRES  
 IMPACT AREA: 0.00 ACRES



TYPICAL LOT DETAIL  
SCALE: 1" = 50 FEET

DISTANCE BETWEEN DRIVE AND LOT LINE VARIES, WILL BE FRACTIONATED IN FUTURE PLANS



### SIGN QUANTITIES

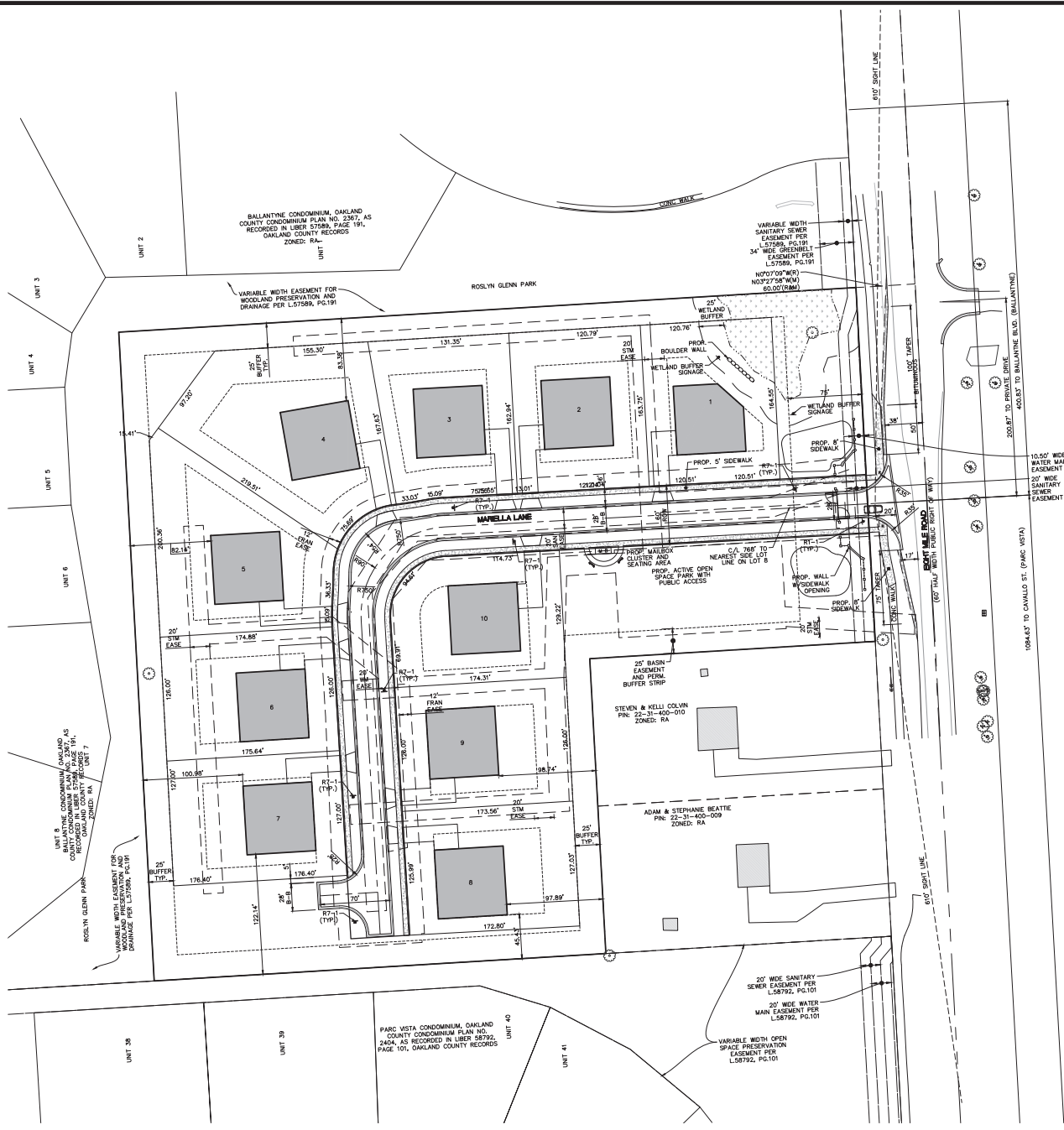
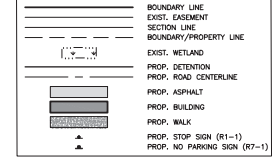
SIGN TYPE	SIGN SIZE	QUANTITY
R1-1	30"x30"	1
R7-1	18"x12"	5

NOTE: ALL TRAFFIC SIGNAGE WILL COMPLY WITH CURRENT LIMITEDS STANDARDS.

### PAVING QUANTITIES

PAVING	Quantity	Unit
Bituminous Surface Course (1.5")	3,004	S.Y.
Bituminous Base Course (2")	3,004	S.Y.
8" 21AA Aggregate Base	3,602	S.Y.
Concrete Curb & Gutter	1,922	L.F.
Concrete Sidewalk (4" conc.)	9,933	S.F.

### LEGEND

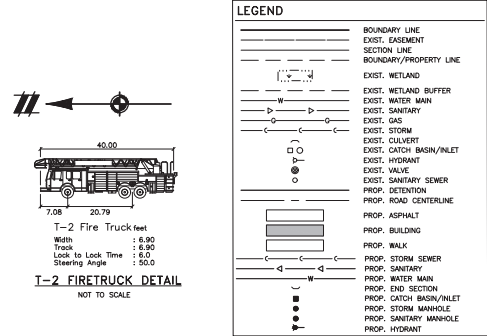
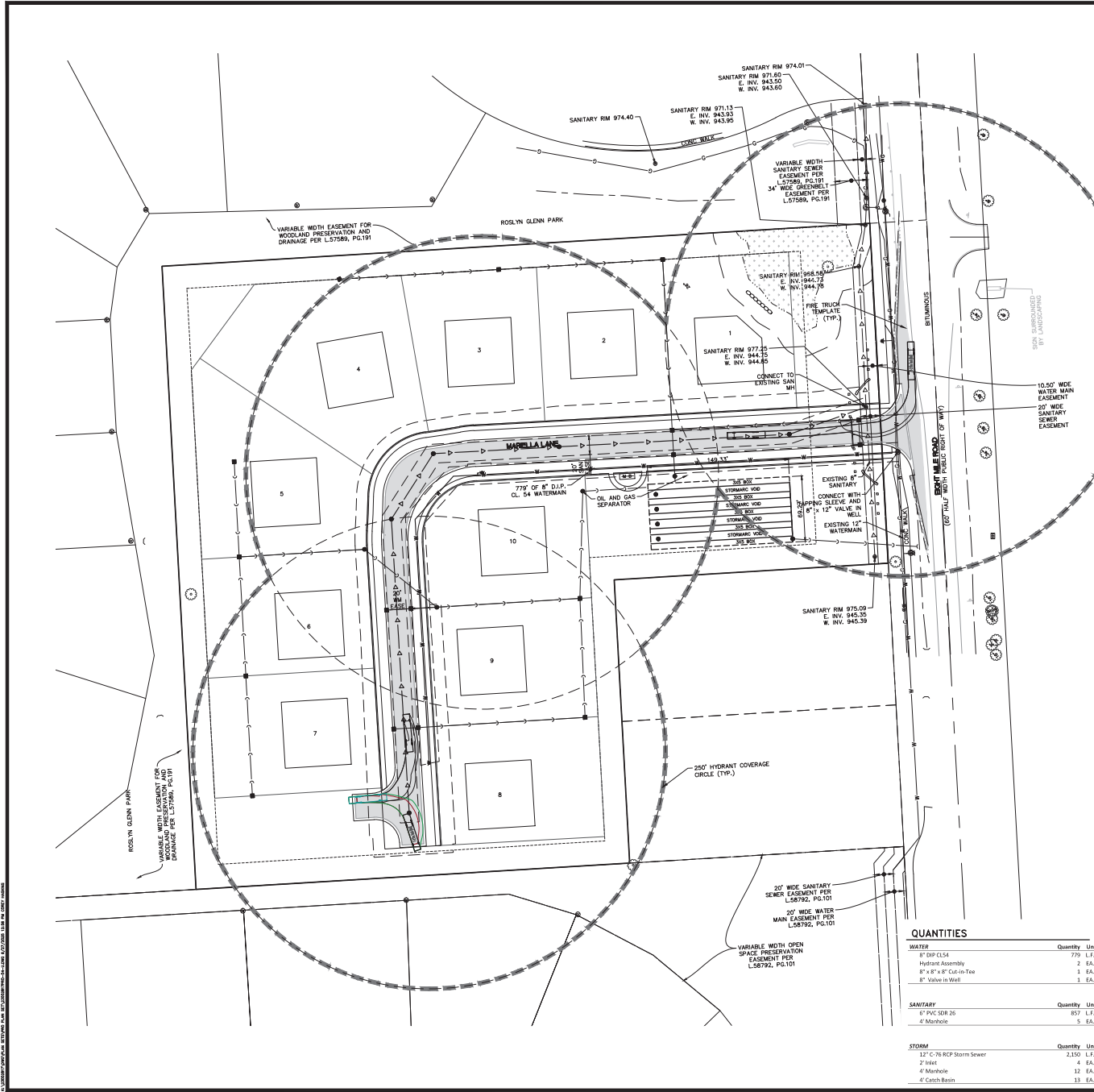


**ATWELL**  
 CIVIL ENGINEERING & ARCHITECTURE  
 1000 W. WASHINGTON ST., SUITE 200  
 ANN ARBOR, MI 48106  
 TEL: 734.934.4000

CLIENT: BRADGIE BROTHERS, LLC  
 PROJECT: MARELLA ESTATES CONCEPTUAL PLANNED ZONING OVERLAY  
 LOCATION: TOWN OF NORTH, RANGE 08 EAST, CITY OF NOV, OAKLAND COUNTY, MICHIGAN

DATE: NOVEMBER 6, 2024  
 02/28/2025 PER CITY  
 06/27/2025 PER CITY

REVISIONS:  
 SCALE: 0 25 50  
 1" = 50 FEET  
 DR. SK. CH. CR.  
 P.L. CR.  
 BOOK ---  
 JOB 23002817  
 SHEET NO. 04



**UTILITY NOTES**

1. THE DEVELOPMENT WILL BE SERVED BY PUBLIC WATER AND SANITARY SEWER. THE LOCATIONS OF THE EXISTING WATER AND SEWER AS SHOWN ARE APPROXIMATE. FIRE PROTECTION IN ACCORDANCE WITH CITY OF NOVI STANDARDS WILL BE DEPICTED ON SUBSEQUENT SUBMITTALS.
2. ALL FIRE HYDRANTS AND WATER MAINS SHALL BE INSTALLED AND IN SERVICE PRIOR TO ABOVE FOUNDATION BUILDING CONSTRUCTION.
3. WATERMAIN AND SANITARY SEWER SHALL BE LOCATED APPROXIMATELY 8.5' OFF THE BACK OF CURB.
4. IT IS ASSUMED THAT THE BALANTINE PROJECT DIRECTLY TO THE NORTH & EAST WILL BE COMPLETED PRIOR TO THE START OF CONSTRUCTION OF THIS PROJECT.
5. WATERMAIN SHALL BE 8" D.I.P.
6. SANITARY SEWER SHALL BE 8" SDR-26
7. SANITARY SEWER LEADS SHALL BE SDR-23.5
8. 6-INCH HYDRANT LEADS ARE ALLOWED FOR LEADS LESS THAN OR EQUAL TO 25 FEET IN LENGTH. 8-INCH LEADS ARE REQUIRED FOR LEADS GREATER THAN 25 FEET IN LENGTH.
9. ALL GATE VALVES 6" OR LARGER SHALL BE PLACED IN A WELL WITH THE EXCEPTION OF A HYDRANT SHUT OFF VALVE. A VALVE SHALL BE PLACED IN A BOX FOR WATER MAIN SMALLER THAN 6".
10. SANITARY LEADS SHALL BE BURIED AT LEAST 5 FEET DEEP WHERE UNDER THE INFLUENCE OF PAVEMENT.

**SANITARY BASIS OF DESIGN**

Service Areas	On site:	9.36 Acres
	Off site:	0 Acres
	Total:	9.36 Acres
Number of Single Family Units	On site:	10 REUs
	Off site (assuming 4 REUs acre):	0 REUs
	Total:	10 REUs
No. of Users per Unit:		3.2 persons
Total Expected Population Served:		32 persons
Average Daily Flow (per capita):		100 G.P.D.
Design Population Denialtes:		3.4 persons/acre
Peaking Factor:		4.00 (FOP less than 500)
Average Flow:	POP * 100 =	0.0032 M.G.D.
	=	2.22 G.P.M.
	=	0.0050 C.F.S.
Peak Flow:	0.0032 * 4.00 =	0.0128 M.G.D.
	=	8.89 G.P.M.
	=	0.0198 C.F.S.
Pipe Capacity:	8 in. diameter	A= 0.346 sf
	0.40% slope	R= 0.167 ft
	0.013 Mannings 'n'	
	Mannings Capacity =	0.786 C.F.S.
	Velocity Flowing Full =	2.20 F.P.S.

**WATER BASIS OF DESIGN**

Number of Single Family Units:		10 Units
Residential Equivalent Units:		10 REUs
No. of Users per Unit:		3.2 persons
Total Expected Population Served:		32 persons
Average Daily Flow (per capita):		100 G.P.D.
Peaking Factor:		2.50 (Per Nov Code for Population less)
Average Flow:	POP * 100 =	0.0032 M.G.D.
Peak Flow:	0.0032 * 2.50 =	0.0080 M.G.D.

**QUANTITIES**

WATER	Quantity	Unit
8" DIP C154	779	L.F.
Hydrant Assembly	2	EA.
8" x 8" Cast-iron Tee	1	EA.
8" Valve in Wall	1	EA.
SANITARY	Quantity	Unit
8" PVC SDR 26	897	L.F.
4" Manhole	5	EA.
STORM	Quantity	Unit
12" C-76 RCP Storm Sewer	2,150	L.F.
2' Manhole	4	EA.
4' Manhole	12	EA.
4' Catch Basin	13	EA.

Know what's below.  
Call before you dig.

THE LOCATION OF EXISTING UTILITY LINES SHALL BE SHOWN BY SYMBOLS ONLY AND WILL NOT BE GUARANTEED. THE CONTRACTOR SHALL DETERMINE THE EXACT LOCATION OF ALL UTILITIES. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY AND ALL DAMAGES WHICH MAY BE INCURRED BY THE CONTRACTOR DURING THE WORK OF EXPOSURE ENDED IN THE WORK OF ANY UTILITY STRUCTURES OR OF ANY OTHER PERSONS.

NOTICE: OPERATIONS SHALL BE THE SOLE RESPONSIBILITY OF THE CONTRACTOR. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF EXISTING UTILITIES. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF EXISTING UTILITIES. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF EXISTING UTILITIES.

ATWELL  
CONCEPTUAL PLANNED  
REZONING OVERLAY

SECTION 31  
TOWN OF NORTH, RANGE 08 EAST  
CITY OF NOVI  
OAKLAND COUNTY, MICHIGAN

BRADJOE BROTHERS, LLC  
MARELLA ESTATES  
CONCEPTUAL PLANNED  
REZONING OVERLAY

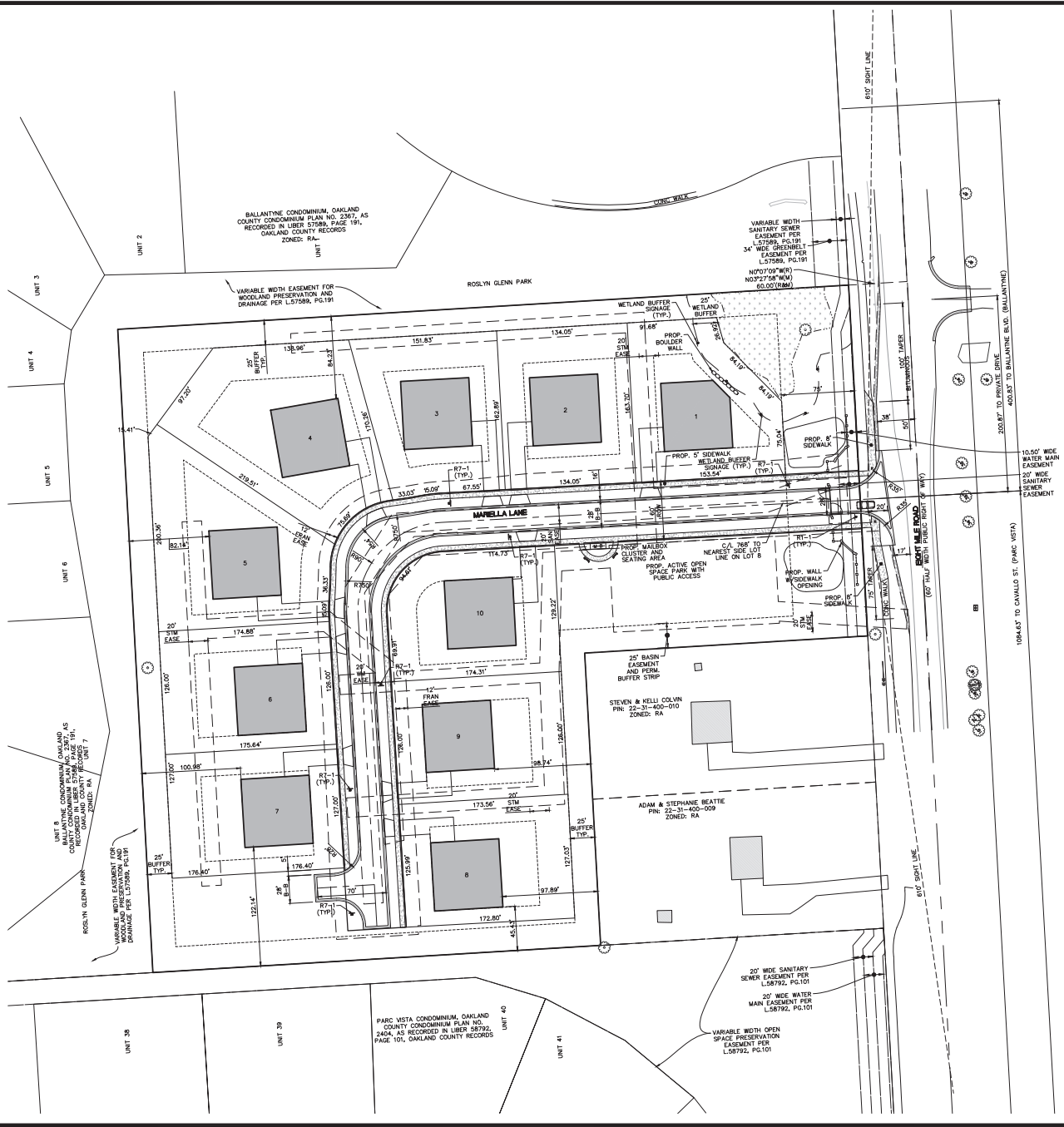
UTILITY PLAN

DATE: NOVEMBER 6, 2024  
02/28/2025 PER CITY  
06/27/2025 PER CITY

REVISIONS

SCALE: 0 25 50  
1" = 50 FEET

DR. SK. CR  
P.L. CR  
BOOK ---  
JOB 23002817  
SHEET NO. 05

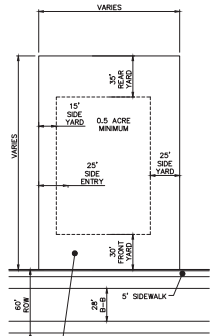


**LAYOUT NOTES**

1. THE DEVELOPMENT WILL CONTAIN PRIVATE ROADS. THE PROPOSED CONNECTION WILL BE COORDINATED WITH THE ROAD COMMISSION OF OAKLAND COUNTY.
2. THE ROAD WILL NOT BE GATED.
3. LANDSCAPE BUFFERS WILL BE PROVIDED ON THE SIDES OF THE DEVELOPMENT AS SHOWN BUFFERING THE PROPERTY FROM THE ADJACENT USES.
4. LANDSCAPING SHOWN IS PRELIMINARY AND SUBJECT TO CHANGE.
5. ALL SIDEWAYS AND SIDEWALK RAMPS SHALL BE ADA COMPLIANT AND SHALL COMPLY WITH BARRIER FREE DESIGN STANDARDS.
6. NO ON-STREET PARKING IS PROPOSED. UNIT GARAGE AND DRIVER PARKING EXCEEDS CITY PARKING REQUIREMENTS. NO PARKING SIGNAGE TO BE PLACED IN AREAS OF ROAD CURVE RADIUS LESS THAN 230'.
7. ALL ON-SITE SIGNAGE AND PAVEMENT MARKINGS SHALL BE IN COMPLIANCE WITH THE LIMITEDS.
8. PROPOSED BUILDING FACADES WILL HAVE A MINIMUM OF 30R BRICK OR STONE.
9. THE TOTAL PROPOSED FLOOR SQUARE FOOTAGE OF EACH DWELLING UNIT VARIES FROM 3,200 TO 4,300 SQUARE FEET.
10. SIGHT DISTANCE TRIANGLES SHOWN FOR 1.5' EYE HEIGHT IN ACCORDANCE WITH MOST TRAFFIC AND SAFETY STANDARDS APRIL 22, 2015 (2011 ASHOTO, TABLE # - 6, 9-38). SIGHT DISTANCE FOR SPEED LIMIT FOR 55 MPH AND 2 LINES OF TRAFFIC IS 610'.
11. CITY OF NOV HAS NO RESPONSIBILITY TO IMPROVE OR MAINTAIN THE PRIVATE STREETS CONTAINED WITHIN OR PRIVATE STREETS PROVIDING ACCESS TO THE PROPERTY DESCRIBED IN THIS PLAN.

**WETLAND SUMMARY**

TOTAL WETLAND AREA: 0.11 ACRES  
 TOTAL WETLAND IMPACT AREA: 0.00 ACRES



**TYPICAL LOT DETAIL**  
 SCALE: 1" = 50 FEET

LOT NO.	AREA (SF)	LOT WIDTH	D/W RATIO
1	21888.32	153.54'	1.1
2	21889.46	142.00'	1.2
3	21891.35	122.02'	1.3
4	30496.17	98.53'	1.7
5	28953.05	98.67'	1.7
6	22082.74	126'	1.4
7	22354.11	127'	1.4
8	21909.57	126.17'	1.4
9	21916.09	126'	1.4
10	21883.10	129.91'	1.3

AVERAGE LOT AREA: 23,622 SF  
 TOTAL LOT AREA: 236,216 SF

\* LOT WIDTH IS MEASURED FROM SIDE LOT LINE TO SIDE LOT LINE AT FRONT SETBACK



**SIGN QUANTITIES**

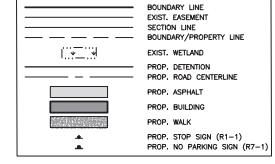
SIGN TYPE	SIGN SIZE	QUANTITY
R1-1	30"x30"	1
R7-1	18"x12"	5

NOTE: ALL TRAFFIC SIGNAGE WILL COMPLY WITH CURRENT LIMITEDS STANDARDS.

**PAVING QUANTITIES**

PAVING	Quantity	Unit
Bituminous Surface Course (1.5")	3,004	S.Y.
Bituminous Base Course (2")	3,004	S.Y.
8" 21AA Aggregate Base	3,602	S.Y.
Concrete Curb & Gutter	1,922	L.F.
Concrete Sidewalk (4" conc.)	9,933	S.F.

**LEGEND**



**ATWELL**  
 CIVIL ENGINEERING & ARCHITECTURE  
 1000 W. WASHINGTON ST., SUITE 200  
 ANN ARBOR, MI 48106  
 TEL: 734.924.4000

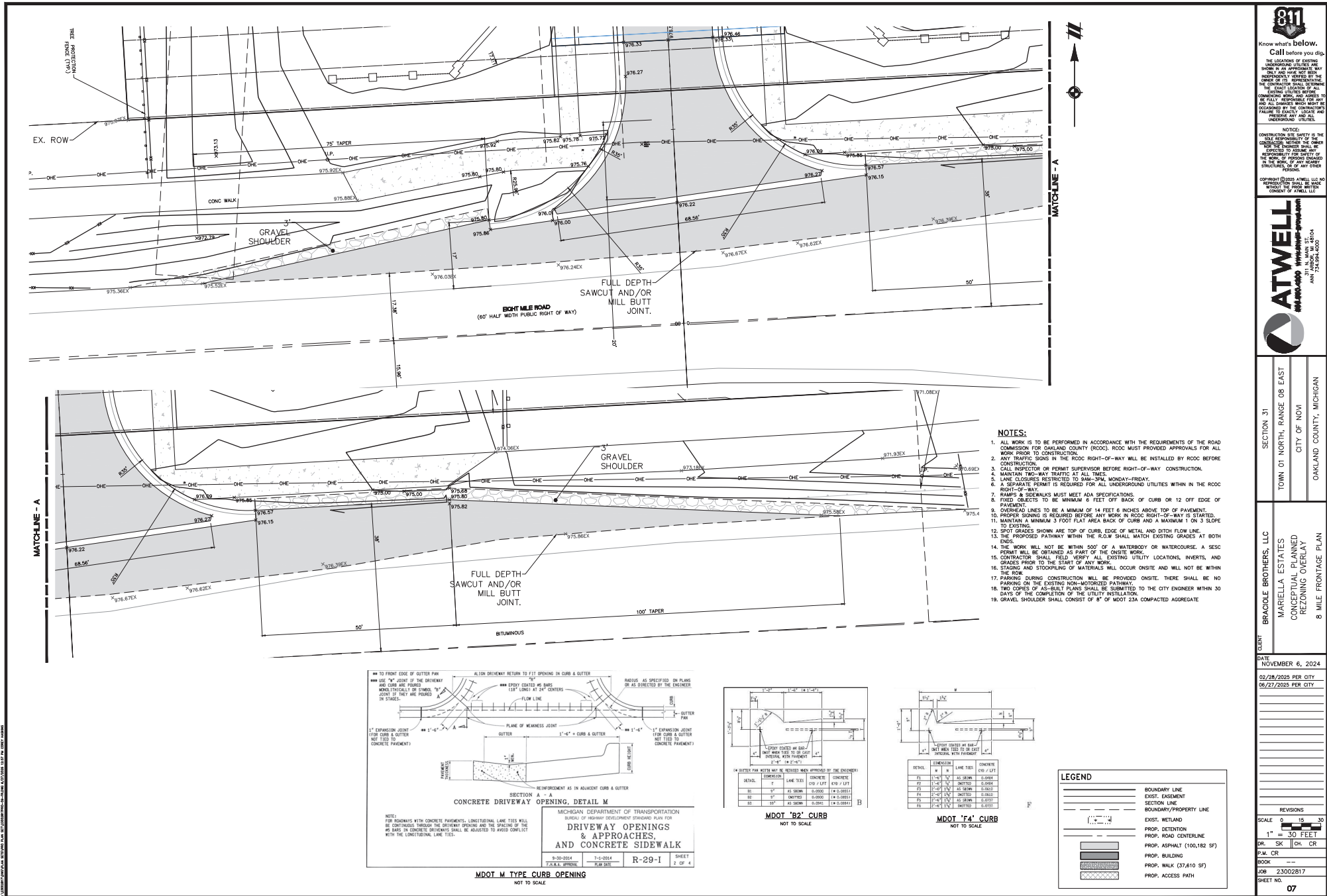
CLIENT: BRADGIE BROTHERS, LLC  
 PROJECT: MARELLA ESTATES CONCEPTUAL PLANNED ZONING OVERLAY  
 SHEET NO. 04

DATE: NOVEMBER 6, 2024  
 02/28/2025 PER CITY  
 06/27/2025 PER CITY

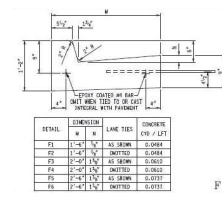
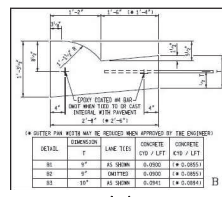
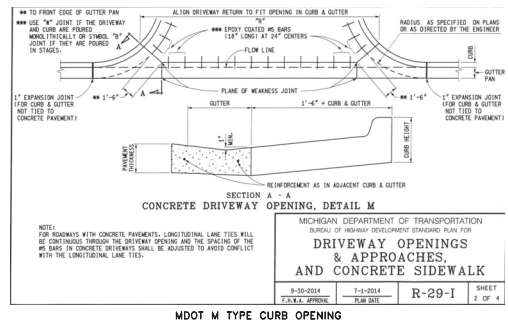
SCALE: 0 25 50  
 1" = 50 FEET

REVISIONS:  
 No. | Description | Date | By | CR

BOOK: ---  
 JOB: 23002817  
 SHEET NO. 04



- NOTES:**
1. ALL WORK IS TO BE PERFORMED IN ACCORDANCE WITH THE REQUIREMENTS OF THE ROAD COMMISSION FOR OAKLAND COUNTY (RCOC). RCOC MUST PROVIDE APPROVALS FOR ALL WORK PRIOR TO CONSTRUCTION.
  2. ANY TRAFFIC SIGNS IN THE RCOC RIGHT-OF-WAY WILL BE INSTALLED BY RCOC BEFORE CONSTRUCTION.
  3. CALL INSPECTOR OR PERMIT SUPERVISOR BEFORE RIGHT-OF-WAY CONSTRUCTION.
  4. MAINTAIN TWO-WAY TRAFFIC AT ALL TIMES.
  5. LANE CLOSURES RESTRICTED TO 9AM-5PM, MONDAY-FRIDAY.
  6. A SEPARATE PERMIT IS REQUIRED FOR ALL UNDERGROUND UTILITIES WITHIN IN THE RCOC RIGHT-OF-WAY.
  7. RAMPS & SIDEWALKS MUST MEET ADA SPECIFICATIONS.
  8. FIXED OBJECTS TO BE MINIMUM 6 FEET OFF BACK OF CURB OR 12' OFF EDGE OF PAVEMENT.
  9. OVERHEAD LINES TO BE A MINIMUM OF 14 FEET 6 INCHES ABOVE TOP OF PAVEMENT.
  10. PROPER SIGNING IS REQUIRED BEFORE ANY WORK IN RCOC RIGHT-OF-WAY IS STARTED.
  11. MAINTAIN A MINIMUM 3 FOOT FLAT AREA BACK OF CURB AND A MAXIMUM 1 ON 3 SLOPE TO EXISTING.
  12. SPOT GRADES SHOW ARE TOP OF CURB, EDGE OF METAL AND DITCH FLOW LINE.
  13. THE PROPOSED PATHWAY WITHIN THE R.O.W SHALL MATCH EXISTING GRADES AT BOTH ENDS.
  14. THE WORK WILL NOT BE WITHIN 500' OF A WATERBODY OR WATERCOURSE. A SECC PERMIT WILL BE OBTAINED AS PART OF THE ONSITE WORK.
  15. CONTRACTOR SHALL VERIFY ALL EXISTING UTILITY LOCATIONS, INVERTS, AND GRADES PRIOR TO THE START OF ANY WORK.
  16. PARKING DURING CONSTRUCTION WILL BE OCCUR ONSITE AND WILL NOT BE WITHIN THE R.O.W.
  17. PARKING DURING CONSTRUCTION WILL BE OCCUR ONSITE. THERE SHALL BE NO PARKING ON THE EXISTING NON-MOTORIZED PATHWAY.
  18. TWO COPIES OF AS-BUILT PLANS SHALL BE SUBMITTED TO THE CITY ENGINEER WITHIN 30 DAYS OF THE COMPLETION OF THE UTILITY INSTALLATION.
  19. GRAVEL SHOULDER SHALL CONSIST OF 8" OF MDT 233 COMPACTED AGGREGATE.



**LEGEND**

SYMBOL	DESCRIPTION
---	BOUNDARY LINE
---	EXIST. EASEMENT
---	SECTION LINE
---	BOUNDARY/PROPERTY LINE
---	EXIST. WETLAND
---	PROP. DETENTION
---	PROP. ROAD CENTERLINE
---	PROP. ASPHALT (100,182 SF)
---	PROP. BUILDING
---	PROP. WALK (37,610 SF)
---	PROP. ACCESS PATH

**811**  
Know what's below.  
Call before you dig.  
THE LOCATION OF EXISTING UNDERGROUND UTILITIES MAY VARY AND HAVE NOT BEEN INDICATED BY THESE PLANS. THE CONTRACTOR SHALL DETERMINE THE EXACT LOCATION OF ALL UTILITIES PRIOR TO ANY CONSTRUCTION WORK AND BE FULLY RESPONSIBLE FOR ANY DAMAGE TO EXISTING UTILITIES. ALL DAMAGES MUST BE REPAIRED BY THE CONTRACTOR AT HIS OWNERS RISK AND ALL NECESSARY PERMITS AND ALL NECESSARY UTILITIES.

**NOTICE:**  
CONSTRUCTION OF THIS PROJECT IS THE SOLE RESPONSIBILITY OF THE CONTRACTOR. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM THE ROAD COMMISSION FOR OAKLAND COUNTY (RCOC) PRIOR TO CONSTRUCTION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM THE ROAD COMMISSION FOR OAKLAND COUNTY (RCOC) PRIOR TO CONSTRUCTION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM THE ROAD COMMISSION FOR OAKLAND COUNTY (RCOC) PRIOR TO CONSTRUCTION.

**ATWELL**  
CONSULTING ENGINEERS  
1000 W. WASHINGTON ST. SUITE 200  
ANN ARBOR, MI 48104  
PH: 734.934.4000

**BRADCOE BROTHERS, LLC**  
MARELLA, ESTATES  
CONCEPTUAL PLANNED  
REZONING OVERLAY  
8 MILE FRONTAGE PLAN

SECTION 31  
TOWN 01 NORTH, RANGE 08 EAST  
CITY OF NOV  
OAKLAND COUNTY, MICHIGAN

DATE: NOVEMBER 6, 2024  
02/28/2025 PER CITY  
06/27/2025 PER CITY

SCALE: 0 15 30  
1" = 30 FEET

REVISONS

SCALE: 0 15 30  
1" = 30 FEET

BOOK ---  
JOB 23002817  
SHEET NO. 07





Landscape Buffer



Mailbox & Seating Area



Active Open Space Park



Entrance Sign and Landscape

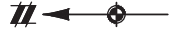


Seating Area and Trash Receptacle



Basketball Court

Proposed ITC Park improvements.  
See letter for additional details



Know what's below.  
Call before you dig.  
THE LOCATION OF EXISTING AND PROPOSED UTILITIES IS THE PROPERTY OF THE UTILITY OWNERS AND SHALL BE DETERMINED BY THE UTILITY OWNERS. THE CONTRACTOR SHALL DETERMINE THE EXACT LOCATION OF ALL EXISTING UTILITIES BEFORE CONSTRUCTION WORK AND SHALL BE FULLY RESPONSIBLE FOR ANY AND ALL DAMAGES WHICH MUST BE REPAIRED BY THE CONTRACTOR. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND RECORDS AND ALL NECESSARY UTILITIES.

NOTICE: CONSTRUCTION SHALL BE THE SOLE RESPONSIBILITY OF THE CONTRACTOR. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND RECORDS AND ALL NECESSARY UTILITIES. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND RECORDS AND ALL NECESSARY UTILITIES.

COPYRIGHT © 2025 ATWELL LLC. NO REPRODUCTION SHALL BE MADE WITHOUT THE WRITTEN CONSENT OF ATWELL LLC.

**ATWELL**  
LANDSCAPE ARCHITECTURE  
ANY LANDSCAPE DESIGN

SECTION 31  
TOWN 01 NORTH, RANGE 08 EAST  
CITY OF NOV  
OAKLAND COUNTY, MICHIGAN

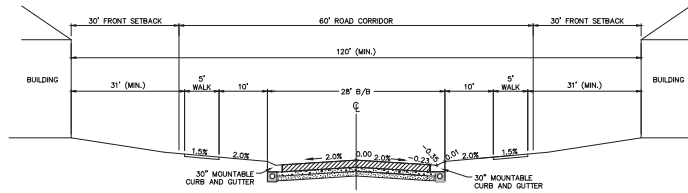
CLIENT: BRADOLE BROTHERS, LLC  
MARELLA ESTATES  
CONCEPTUAL PLANNED  
REZONING OVERLAY  
CONCEPTUAL AMENITIES PLAN

DATE: NOVEMBER 6, 2024  
02/28/2025 PER CITY  
06/27/2025 PER CITY

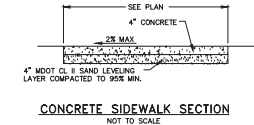
REVISIONS

SCALE 0 30 60  
1" = 60 FEET

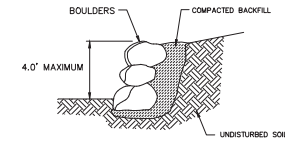
DR. SK. CH. CR  
P.M. CR  
BOOK ---  
JOB 23002817  
SHEET NO. 08



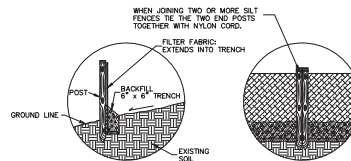
TYPICAL ROAD CROSS SECTION  
NO SCALE



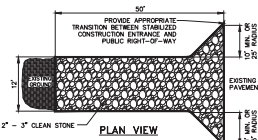
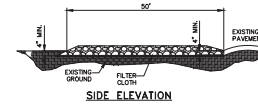
CONCRETE SIDEWALK SECTION  
NOT TO SCALE



BOULDER WALL  
NO SCALE

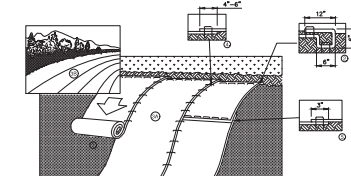


GEOTEXTILE SILT FENCE  
NO SCALE



- CONSTRUCTION SPECIFICATIONS
1. STONE - USE COARSE AGGREGATE (2 - 3 INCH STONE)
  2. LENGTH - 50' MIN.
  3. THICKNESS - NOT LESS THAN EIGHT (8) INCHES.
  4. WIDTH - 12' MIN.

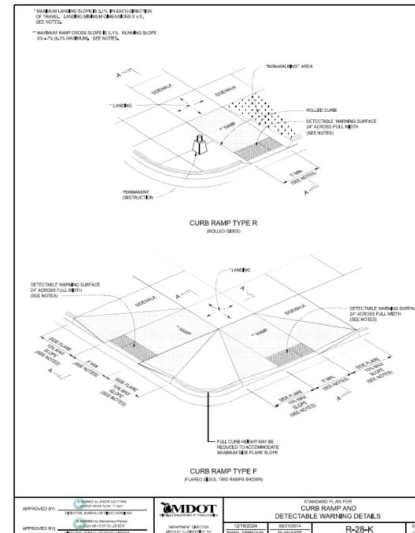
STABILIZED CONSTRUCTION ENTRANCE AND STONE LAYDOWN  
NOT TO SCALE



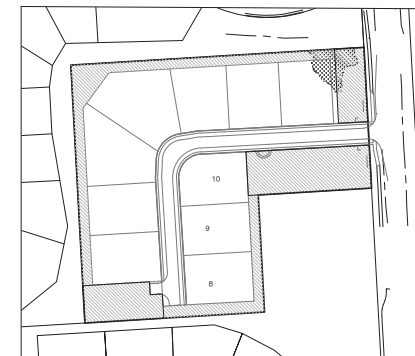
1. PREPARE SOIL BEFORE INSTALLING BLANKETS, INCLUDING ANY NECESSARY APPLICATION OF LIME, FERTILIZER, AND SEED.
2. BEGIN AT THE TOP OF THE SLOPE BY ANCHORING THE BLANKET IN A 4\"/>

- NOTES:
1. IN LOOSE SOIL CONDITIONS, THE USE OF STAPLE OR STAKE LENGTHS GREATER THAN 4\"/>

EROSION CONTROL BLANKET (SLOPE INSTALLATION)  
NO SCALE



APPROVED BY:	<b>MDOT</b>	DESIGNED BY:	BRADIGLE BROTHERS, LLC
APPROVED BY:		DATE:	11/06/2024
		PROJECT:	R-28-K
		SCALE:	1" = 10'



OPEN SPACE EXHIBIT  
SCALE: 1" = 150 FEET

TOTAL OPEN SPACE = 2.65 ACRES  
TOTAL USABLE OPEN SPACE = 1.55 ACRES

PRELIMINARY - NOT FOR CONSTRUCTION



Know what's below.  
Call before you dig.

THE LOCATION OF EXISTING KNOWLEDGE RECORDS MAY ONLY AND HAVE NOT BEEN INDEPENDENTLY VERIFIED BY THE CONTRACTOR SHALL DETERMINE THE EXACT LOCATION OF ALL UTILITIES. THE CONTRACTOR SHALL BE FULLY RESPONSIBLE FOR ANY AND ALL DAMAGES WHICH MUST BE FOLLOWED BY THE CONTRACTOR PRESERVE ANY AND ALL INFORMATION UTILITIES.

NOTICE: OPERATIONS ARE TO BE THE CONTRACTOR'S RESPONSIBILITY. THE CONTRACTOR SHALL BE RESPONSIBLE TO OBTAIN ALL NECESSARY PERMITS AND TO OBTAIN ALL NECESSARY INFORMATION FROM THE LOCAL GOVERNMENT OF KATONAH, ILL. BEFORE THE WORK OF RECORDS ENGRAVED IN THE SOLE OF ANY STREET STRUCTURES OF ANY OTHER RECORDS.

COPYRIGHT © 2024 ATWELL LLC. NO REPRODUCTION SHALL BE MADE WITHOUT THE WRITTEN CONSENT OF ATWELL LLC.



SECTION 31  
TOWN OF NORTH, RANGE 08 EAST  
CITY OF NOV  
OAKLAND COUNTY, MICHIGAN

CLIENT  
BRADIGLE BROTHERS, LLC  
MARELLA, ESTATES  
CONCEPTUAL PLANNED  
REZONING OVERLAY  
STANDARD DETAILS

DATE  
NOVEMBER 6, 2024  
02/28/2025 PER CITY  
06/27/2025 PER CITY

REVISIONS  
SCALE 0  
AS NOTED

DR. SK CH. CR  
BOOK  
JOB 23002817  
SHEET NO. 09

**STORM ARC INSTALLATION SPECIFICATIONS**

- STORM ARC SHALL BE INSTALLED IN ACCORDANCE WITH ASTM C891 STANDARD PRACTICE FOR INSTALLATION OF UNDERGROUND PRE-CAST CONCRETE UTILITY STRUCTURES. THE FOLLOWING ADDITIONS AND/OR EXCEPTIONS SHALL APPLY:
- IT IS THE RESPONSIBILITY OF THE INSTALLING CONTRACTOR TO ENSURE THAT PROPER/ADEQUATE EQUIPMENT IS USED TO SET/INSTALL THE MODULES.
- THE AGGREGATE FOUNDATION HAS BEEN DESIGNED BASED ON THE FOLLOWING ASSUMPTIONS. THESE ASSUMPTIONS WILL NEED TO BE VERIFIED BY A GEOTECHNICAL ENGINEER WHICH WILL NEED TO BE EMPLOYED BY THE OWNER.
  - A QUALIFIED GEOTECHNICAL ENGINEER WILL BE EMPLOYED, BY THE OWNER, TO PROVIDE ASSISTANCE IN EVALUATING THE EXISTING SOIL CONDITIONS BELOW THE PROPOSED ENGINEERED STONE FOUNDATION. IF A STONE FOUNDATION DESIGN IS TO BE USED, THE BEARING CAPACITY OF THE SOILS BELOW THE STONE WILL NEED TO BE EVALUATED FOR ACCEPTABILITY. IF THE SOIL ARE DETERMINED TO BE UNACCEPTABLE, THE STONE FOUNDATION MAY NOT BE AN OPTION FOR THIS LOCATION.
  - A QUALIFIED GEOTECHNICAL ENGINEER WILL BE EMPLOYED, BY THE OWNER, TO EVALUATE THE SOURCE OF STONE AGGREGATES THAT WILL BE PLACED ON PROPERLY COMPACTED SOILS. THE AGGREGATE BASE COURSE FOR WHICH THE STORM ARC SYSTEM WILL BEAR DIRECTLY ON SHALL CONSIST OF A 9" THICK BED OF 3/4" TO 1-1/2" DIAMETER ANGULAR STONE, WELL COMPACTED AND SEATED, WITH NO FINES, AND A 9" THICK BED OF 3" ANGULAR AGGREGATE (SEE DETAIL 2005/2.0 ON SHEET 2). PLEASE NOTE THAT THESE ARE ONLY MINIMUM RECOMMENDATIONS AND A QUALIFIED GEOTECHNICAL ENGINEER SHALL BE USED TO DETERMINE THE EXACT REQUIREMENTS FOR THE LOCATIONS THAT THE STORM ARC SYSTEM IS TO BE LOCATED.
  - THE CONTRACTOR SHALL REMOVE ANY AND ALL EXPANDABLE OR COLLAPSIBLE SOILS AT THE DIRECTION OF A QUALIFIED GEOTECHNICAL ENGINEER.
  - THE AGGREGATE FOUNDATION SHALL BE INSTALLED SUCH THAT THE AGGREGATE EXTENDS A MINIMUM OF 1'-0" PAST THE OUTSIDE OF THE SYSTEM (SEE DETAIL 2005/2.0).
  - THE 3/4" TO 1-1/2" AGGREGATE SHALL BE COMPACTED USING A VIBRATING ROLLER OR PLATE COMPACTOR CAPABLE OF APPLYING A MINIMUM OF 4,000 LBS OF FULL DYNAMIC FORCE TO ACHIEVE A FLAT SURFACE.
  - DISK, DRY, AND COMPACT THE TOP 8" OF THE SUBGRADE SOILS TO 95% OF THE STANDARD DRY DENSITY AND 110% OPTIMUM MOISTURE CONTENT.
  - AGGREGATE SHALL BE GRADED WITHIN +/-1/4" OF THE GRADE SHOWN ON THE PLANS.
  - THE STORM ARC MODULES SHALL BE PLACED SUCH THAT THE MAXIMUM SPACE BETWEEN ADJACENT MODULES DOES NOT EXCEED 3/4" (SEE DETAIL 2004/2.0). IF THE SPACE EXCEEDS 3/4" THE MODULES SHALL BE RESET WITHIN APPROPRIATE ADJUSTMENT MADE TO LINE AND GRADE TO BRING THE SPACE INTO SPECIFICATION.
  - STORM ARC MODULES ARE NOT WATER TIGHT. IF A WATER TIGHT SOLUTION IS REQUIRED, CONTACT STORM ARC FOR RECOMMENDATIONS. THE WATER TIGHT APPLICATION IS TO BE PROVIDED AND IMPLEMENTED BY THE CONTRACTOR. THE CONTRACTOR IS RESPONSIBLE TO ENSURE THAT THE SELECTED WATER TIGHT SOLUTION PERFORMS AS SPECIFIED BY THE MANUFACTURER.
  - ALL EXTERIOR ROOF AND EXTERIOR VERTICAL WALL JOINTS BETWEEN ADJACENT STORM ARC MODULES SHALL BE SEALED WITH 8" WIDE, PRE-FORMED, COOL-APPLIED, SELF-ADHERING ELASTOMERIC RESIN, BONDED TO A MOVEN, HIGHLY PUNCTURE RESISTANT POLYMER WRAP, CONFORMING TO ASTM C891 AND SHALL BE INTEGRATED WITH PRIMER SEALANT AS APPROVED BY STORM ARC. THE JOINT WRAP DOES NOT PROVIDE A WATER TIGHT SEAL. THE SOLE PURPOSE OF THE JOINT WRAP IS TO PROVIDE A SILT AND SOIL TIGHT SYSTEM. THE ADHESIVE EXTERIOR JOINT WRAP SHALL BE INSTALLED ACCORDING TO THE FOLLOWING INSTALLATION INSTRUCTIONS:
    - USE A BRUSH OR WET CLOTH TO THOROUGHLY CLEAN THE OUTSIDE SURFACE AT THE POINT WHERE THE JOINT WRAP IS TO BE APPLIED.
    - A RELEASE PAPER PROTECTS THE ADHESIVE SIDE OF THE JOINT WRAP. PLACE THE ADHESIVE TAPE (ADHESIVE SIDE DOWN) AROUND THE STRUCTURE, REMOVING THE RELEASE PAPER AS YOU GO. PRESS THE JOINT WRAP FIRMLY AGAINST THE STORM ARC MODULE SURFACE WHEN APPLYING.
    - IF THE CONTRACTOR NEEDS TO CANCEL ANY SHIPMENTS, THEY MUST DO SO 48 HOURS PRIOR TO THEIR SCHEDULED ARRIVAL AT THE JOB SITE. IF CANCELED AFTER THAT TIME, PLEASE CONTACT THE PROJECT MANAGER.
    - IF THE STORM ARC MODULE(S) IS DAMAGED IN ANY WAY PRIOR, DURING, OR AFTER INSTALL, GOODFLOW SOLUTIONS, MUST BE CONTACTED IMMEDIATELY TO ASSESS THE DAMAGE AND TO DETERMINE WHETHER OR NOT THE MODULE(S) WILL NEED TO BE REPLACED. IF ANY MODULE ARRIVES AT THE JOBSITE DAMAGED DO NOT UNLOAD IT. CONTACT STORM ARC IMMEDIATELY. ANY DAMAGE NOT REPORTED BEFORE THE TRUCK IS UNLOADED WILL BE THE CONTRACTOR'S RESPONSIBILITY.
    - STORM ARC MODULES CANNOT BE ALTERED IN ANY WAY AFTER MANUFACTURING WITHOUT WRITTEN CONSENT FROM GOODFLOW SOLUTIONS.

**PRODUCT IDENTIFICATION CHART**

- GOODFLOW STORM ARC - GFA
- GOODFLOW CHAMBERS :
- 3.25' TALL X 8' L - GFC - 3
  - 4.25' TALL X 8' L - GFC - 4
  - 5.25' TALL X 8' L - GFC - 5
  - 6.25' TALL X 8' L - GFC - 6
- GOODFLOW END SLABS :
- GOODFLOW 3' TALL END SLAB SOLID "NO KNOCKOUT" - GFS - 3
  - GOODFLOW 3' TALL END SLAB "WITH PIPE KNOCKOUT" - GFS - K/O -3
  - GOODFLOW 4' TALL END SLAB SOLID "NO KNOCKOUT" - GFS - 4
  - GOODFLOW 4' TALL END SLAB "WITH PIPE KNOCKOUT" - GFS - K/O - 4
  - GOODFLOW 5' TALL END SLAB SOLID "NO KNOCKOUT" - GFS - 5
  - GOODFLOW 5' TALL END SLAB "WITH PIPE KNOCKOUT" - GFS - K/O-5
  - GOODFLOW 6' TALL END SLAB SOLID "NO KNOCKOUT" - GFS - 6
  - GOODFLOW 6' TALL END SLAB "WITH PIPE KNOCKOUT" - GFS - K/O - 6
- GOODFLOW HARDWARE :
- GOODFLOW HARDWARE PACKAGE INCLUDING THREADED ROD AND STEEL SLAB ARM - GFH - 1
- GOODFLOW MANHOLE - GFH - 1 - SEE SPECIFICATIONS FOR MANHOLE COVER, RING AND COVER.



**GOODFLOW SOLUTIONS**  
 Manufacturer of Innovative Septic & Drainage Systems  
 23 Ryan Street, Stamford, CT 06907  
 PH: (203) 869-2969  
 F: (203) 869-2977  
 info@goodflow.com

**GENERAL NOTES**

- STORM ARC IS DESIGNED TO BE USED IN CONJUNCTION WITH CONCRETE STRUCTURES TO CREATE ONE OF THE MOST EFFECTIVE AND STRUCTURALLY SUPERIOR STORM WATER DETENTION SYSTEMS IN THE INDUSTRY.
- ARC PARAMETERS:
- ARC DIMENSIONS ARE:
  - MID SECTION TOTAL: TOTAL OVERALL WIDTH- 11'-3 11/16" WIDTH FROM OUTSIDE EDGES OF VERTICAL LEGS- 9'-4" TOTAL OVERALL LENGTH INCLUDING KEYWAYS- 4'-4" ASSEMBLED LENGTH- 4'
  - RIGHT END SECTION: TOTAL OVERALL WIDTH- 11'-3 11/16" WIDTH FROM OUTSIDE EDGES OF VERTICAL LEGS- 9'-4" TOTAL OVERALL LENGTH INCLUDING KEYWAY- 4'-4" ASSEMBLED LENGTH- 4'
  - LEFT END SECTION: TOTAL OVERALL WIDTH- 11'-3 11/16" WIDTH FROM THE OUTSIDE EDGES OF VERTICAL LEGS- 9'-4" TOTAL OVERALL LENGTH INCLUDING KEYWAY- 4'-4" ASSEMBLED LENGTH- 4'
- INSTALLATION DETAILS:
- STORM ARC SECTIONS ARE DESIGNED TO UTILIZE ANY CONCRETE STRUCTURES THAT HAVE SIDES WHERE THE TOPS FORM RIGHT ANGLES AND WIDTH OF 2' OR WIDER SUCH AS CHAMBERS, VAULTS, CONCRETE BLOCKS ETC.
  - STEEL REINFORCED CONCRETE SLABS ARE USED TO SEAL THE END OF THE ARC ROWS. THE SLABS ARE 10' LONG THE HEIGHT OF THE SLAB WHEN INSTALLED WITH ONE OF THE LONG SIDES DOWN IS 12" LOWER THAN THE HEIGHT OF THE FOUNDATIONAL CONCRETE STRUCTURE AND THE THICKNESS OF THE SLAB IS 4"
  - ANY STRUCTURES UTILIZING STORM ARC HAVE TO BE APPROVED AND CERTIFIED BY GOODFLOW SOLUTIONS.
  - ONCE THE FOUNDATIONAL CONCRETE STRUCTURES ARE INSTALLED LEVEL AND WITH THE PROPER SPACING SET THE END SLABS AT THE BEGINNING OF EACH ARC ROW. THE SLABS OVERLAP THE FOUNDATIONAL STRUCTURES BY 3" ON EACH END. THE SLABS ARE SECURED IN PLACE BY HARDWARE PROVIDED WITH EACH SYSTEM
  - AFTER SLABS ARE PLACED AND SECURED START SETTING THE STORM ARCS. EACH ROW BEGINS BY SETTING A LEFT END SECTION. THE BOTTOM OF THE SOLID END WALL SITS ON THE TOP OF THE SLAB.
  - AFTER END SECTIONS ARE SET, PLACE THE MID-SECTION PIECES WITH THE OVER LAPPING TOP & BOTTOM KEYWAYS TIGHTLY TOGETHER. TO CLOSE OFF THE END OF EACH ARC ROW USE THE SAME PROCESS DESCRIBED IN DETAIL #2. THEN LOWER A RIGHT END SECTION TO SEAL THE END OF EACH ROW.
  - ARC MID-SECTIONS CAN BE PRODUCED IN LENGTHS LESS THAN 4'-4" TO ACCOMMODATE ROWS WITH LENGTHS WHERE STANDARD ARC DIMENSIONS DON'T MATCH FOUNDATIONAL STRUCTURE LENGTHS

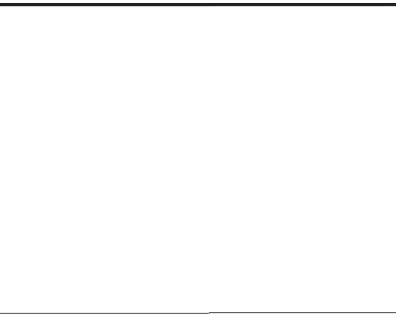
**FILTER FABRIC SPECIFICATIONS / PROCEDURES**

SUPER GEOTEXTILE SGT-160 (6 OZ)			
DESCRIPTION	TEST METHOD	ENGLISH (MARV <sup>2</sup> )	METRIC (MARV <sup>2</sup> )
GRAB TENSILE STRENGTH	ASTM D4632	160 lbs	0.711 kN
GRAB ELONGATION	ASTM D4632	50%	50%
TRAPEZOID TEAR STRENGTH	ASTM D4533	60 lbs	0.267 kN
CBR PUNCTURE RESISTANCE	ASTM D6241	410 lbs	2.823 kN
PERMITTIVITY 4	ASTM D4491	1.5 sec <sup>-1</sup>	1.5 sec <sup>-1</sup>
WATER FLOW 4	ASTM D4491	110 gpm/ft <sup>2</sup>	4480 l/min/m <sup>2</sup>
APPARENT OPENING SIZE (AOS) 384	ASTM D4751	70 Std. U.S. Sieve	0.212 mm
UV RESISTANCE	ASTM D4355	70%/500 hrs	70%/500 hrs

NOTE:

IF NOT UTILIZING A STORM ARC CONCRETE SCOURING PAD AT THE SYSTEM INLET(S), USE 12" WIDE SGT-160 (6 oz) WOVEN FABRIC OR APPROVED EQUAL. CUT A MINIMUM 16' LONG PIECE OF FABRIC PRIOR TO SETTING THE FIRST 2 ROWS OF CHAMBERS ON EITHER SIDE OF THE INLET AND THE END SLAB WITH THE INLET. LAY OUT THE FABRIC SECTION SO THAT A MINIMUM OF 12" OF FABRIC WILL BE UNDER THE CHAMBER WALLS AND END SLAB WHEN THEY ARE SET IN PLACE.

THIS DRAWING WAS PREPARED TO SUPPORT THE PROJECT ENGINEER OF RECORD FOR THE PROPOSED SYSTEM. IT IS THE ULTIMATE RESPONSIBILITY OF THE PROJECT ENGINEER OF RECORD TO ENSURE THAT THE GOODFLOW SYSTEMS DESIGN IS IN FULL COMPLIANCE WITH ALL APPLICABLE LAWS AND REGULATIONS. IT IS THE PROJECT ENGINEER OF RECORD'S RESPONSIBILITY TO ENSURE THAT THE GOODFLOW PRODUCTS ARE DESIGNED IN ACCORDANCE WITH GOODFLOW'S MINIMUM REQUIREMENTS. GOODFLOW DOES NOT APPROVE PLANS, SIZING, OR SYSTEM DESIGNS.



1001 1:6 **STORM ARC - XXXXXX** NTS 1002 1:6 **STORM ARC - GENERAL LAYOUT PLAN VIEW** NTS

**NOTES:**

- BEDDING DEPTH SHALL MEET MANUFACTURERS SPECIFICATIONS.
- THE CITY'S INSPECTING ENGINEERS SHALL VERIFY THE BEARING CAPACITY OF THE NATIVE SOILS TO VERIFY ADEQUATE BEDDING DEPTH IS PROVIDED.
- AGGREGATE POROSITY WILL BE TESTED AND RESULTS WILL BE PROVIDED TO THE CITY'S INSPECTING ENGINEERS

**AREA RESERVED FOR FULL PLAN VIEW OF PROPOSED SYSTEM**

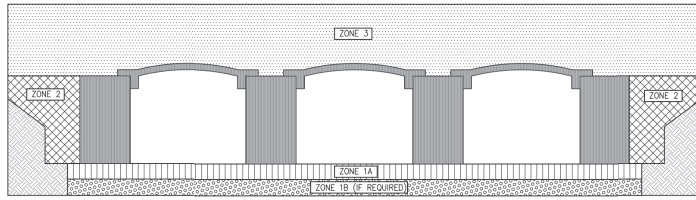
1000 1:6 **STORM ARC - LAYOUT PLAN VIEW** NTS

PROJECT\_NAME:  
 CITY\_STATE:  
 ADDRESS\_1:  
 ADDRESS\_2:

PRECAST SOLID STORM ARC-001	
PROJECT NO: XXXXXXXXX	DATE: XX-XX-XXXX
DESIGNED BY: XXXXXXXXX	CHECKED BY: XXXXXXXXX
SCALE: AS NOTED	SHEET NO: 1 OF 4

### STORM ARC INSTALLATION SPECIFICATIONS

1. THE FILL PLACED AROUND THE STORM ARC MODULES MUST BE DEPOSITED ON BOTH SIDES AT THE SAME TIME AND TO APPROXIMATELY THE SAME ELEVATION. AT NO TIME SHALL THE FILL BEHIND ONE SIDE WALL BE MORE THAN 2'-0" HIGHER THAN THE FILL ON THE OPPOSITE SIDE. BACKFILL SHALL EITHER BE COMPACTED AND/OR VIBRATED TO ENSURE THAT BACKFILL AGGREGATE/STONE MATERIAL IS WELL SEATED AND PROPERLY INTERLOCKED. CARE SHALL BE TAKEN TO PREVENT ANY WEDGING ACTION AGAINST THE STRUCTURE, AND ALL SLOPES WITHIN THE AREA TO BE BACKFILLED MUST BE STEPPED OR SERIATED TO PREVENT WEDGING ACTION. BACKFILL MUST BE FREE-DRAINING MATERIAL. SEE ZONE 2 BACKFILL CHART ON THIS PAGE FOR APPROVED BACKFILL OPTIONS. IF NATIVE EARTH IS SUSCEPTIBLE TO MIGRATION, CONFIRM WITH THE GEOTECHNICAL ENGINEER AND PROVIDE PROTECTION AS REQUIRED (PROVIDED BY OTHERS).
2. DURING PLACEMENT OF MATERIAL OVERTOP THE SYSTEM, AT NO TIME SHALL MACHINERY BE USED OVERTOP THAT EXCEEDS THE DESIGN LIMITATIONS OF THE SYSTEM. WHEN PLACEMENT OF MATERIAL OVERTOP MATERIAL SHALL BE PLACED SUCH THAT THE DIRECTION OF PLACEMENT IS PARALLEL WITH THE OVERALL LONGITUDINAL DIRECTION OF THE SYSTEM WHENEVER POSSIBLE.
3. THE FILL PLACED OVERTOP THE SYSTEM SHALL BE PLACED IN A MINIMUM OF 6" LIFTS. AT NO TIME SHALL MACHINERY OR VEHICLES GREATER THAN THE DESIGN HS-20 LOADING CRITERIA TRAVEL OVERTOP THE SYSTEM WITHOUT THE MINIMUM DESIGN COVERAGE. IF TRAVEL IS NECESSARY OVERTOP THE SYSTEM PRIOR TO ACHIEVING THE MINIMUM DESIGN COVER, IT MAY BE NECESSARY TO REDUCE THE ULTIMATE LOAD/BURDEN OF THE OPERATING MACHINERY SO AS TO NOT EXCEED THE DESIGN CAPACITY OF THE SYSTEM IN SOME CASES. IN ORDER TO ACHIEVE REQUIRED COMPACTION, HAND COMPACTION MAY BE NECESSARY IN ORDER NOT TO EXCEED THE ALLOTTED DESIGN LOADING. SEE CHART FOR TRACKED VEHICLE WIDTH AND ALLOWABLE MAXIMUM PRESSURE PER TRACK.
4. FREE DRAINING AGGREGATE - MINIMUM 85% AGGREGATE RETAINED ON 3/4" SIEVE MAJORITY OF AGGREGATE SIZE BETWEEN 3/4" AND 1 1/2". MAXIMUM OF 5% OF MATERIAL PASSING #3/8 SIEVE.
5. FREE DRAINING, NO FINES, 3" AGGREGATE - MAJORITY OF STONE SIZE IN BETWEEN 1 1/2" AND 3" - VERY SIMILAR TO AASHTO (#1 #2 #3 & #24) STONE AGGREGATE GRADATION.
6. CRUSHED CONCRETE AGGREGATE IS KNOWN TO REACT WITH WATER AND CAN INCREASE THE PH VALUE OF THE GROUND WATER. PRIOR TO USING CRUSHED CONCRETE AGGREGATE IN ZONE 1B, IT IS IMPERATIVE THAT THE USE OF SUCH MATERIAL SHALL BE VERIFIED BY THE ENGINEER OF RECORD AND/OR THE PROJECT GEOTECHNICAL ENGINEER.
7. WHERE DIRECT INLETS ARE TO BE INSTALLED IN THE STORMARC SECTIONS OR A GFC CHAMBER, A 4" CONCRETE ANTI SCOUR PAD OR SIMILAR DEVICE SHALL BE EMPLOYED BELOW THE DIRECT INLET.



BACKFILL DETAIL

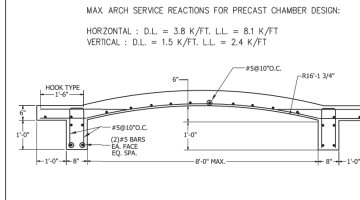
OPTION	APPROVED ZONE 2 BACKFILL OPTIONS	REMARKS
1-1/2" STONE AGGREGATE	THE STONE AGGREGATE SHALL CONSIST OF CLEAN AND FREE DRAINING ANGULAR MATERIAL. THE SIZE OF THIS MATERIAL SHALL HAVE 100% PASSING THE 2" SIEVE WITH 0% TO 10% PASSING THE 3/4" SIEVE (ASTM SIZE #4). THIS AGGREGATE MATERIAL SHALL BE SEPARATED FROM NATIVE MATERIAL BY USING GEOTEXTILE AROUND THE PERIMETER OR THE BACKFILL AS DETERMINED BY THE GEOTECHNICAL ENGINEER.	
SAND	IMPORTED PURE SAND IS PERMITTED TO BE USED AS BACKFILL IF IT IS CLEAN AND FREE DRAINING. THE SAND USED FOR BACKFILLING SHALL HAVE LESS THAN 40% PASSING #40 SIEVE AND LESS THAN 5% PASSING #200 SIEVE. THIS MATERIAL SHALL BE SEPARATED FROM NATIVE MATERIAL BY USING GEOTEXTILE AROUND THE PERIMETER OF THE SAND BACKFILL.	
CRUSHED CONCRETE AGGREGATE	CLEAN, FREE DRAINING CRUSHED UNWORN AGGREGATE MATERIAL CAN BE USED AS CRUSHED BACKFILL FOR THE STORM ARC MODULES. THE SIZE OF THIS MATERIAL SHALL HAVE 100% CONCRETE PASSING THE 1" SIEVE OR 5% PASSING THE #8 SIEVE. THIS MATERIAL SHALL BE SEPARATED FROM NATIVE MATERIAL USING GEOTEXTILE AROUND THE PERIMETER OF THE BACKFILL.	
ROAD PACK	STONE AGGREGATE 100% PASSING THE 1-1/2" SIEVE WITH LESS THAN 12% PASSING THE #200 SIEVE (ASTM SIZE #47). GEOTEXTILE AS PER GEOTECHNICAL ENGINEER RECOMMENDATION.	

ZONE CHART			
ZONES	ZONE DESCRIPTION	REMARKS	
ZONE 1 A	FOUNDATION AGGREGATE	#4 (1 1/2") STONE AGGREGATE (SEE NOTE 4)	
ZONE 1 B (IF REQUIRED)	FOUNDATION AGGREGATE (INFILTRATION NOT ALLOWED)	3" STONE AGGREGATE (SEE NOTE 5)	
ZONE 2	BACKFILL	3" RECYCLED CRUSHED CONCRETE (SEE NOTES 5 & 6)	
ZONE 3	FINAL COVER OVERTOP	UNIFIED SOIL CLASSIFICATION (OW, GP, SW, SP) OR SEE BELOW FOR APPROVED BACKFILL OPTIONS	

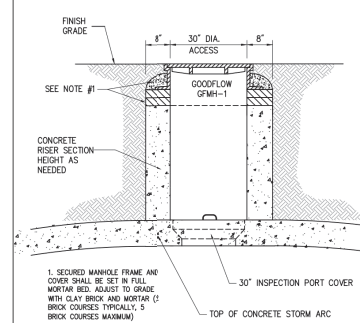
FILL DEPTH	TRACK WIDTH	MAX. VEHICLE HEIGHT (KIP)	MAX. GROUND PRESSURE
12"	12"	51.8	1690 PSF
12"	18"	56.1	1219 PSF
12"	24"	68.1	1111 PSF
12"	30"	76.7	1000 PSF
12"	36"	85.0	924 PSF

NOTES:  
 TRACK LENGTH NOT TO EXCEED 15'-4".  
 ONLY TWO TRACKS PER VEHICLE.

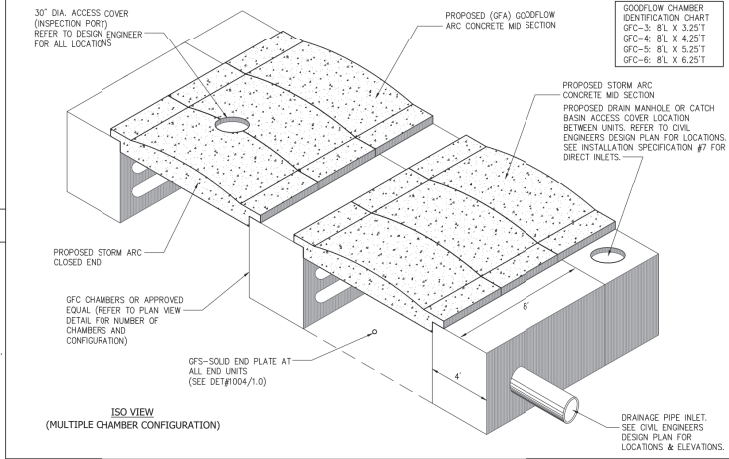


DESIGN NOTES:

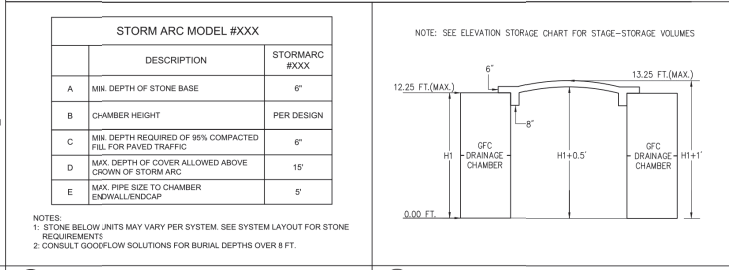
1. DESIGN PER AASHTO 318-14 W/AASHTO HS-20 LL
2. FO @ 28 DAYS = 5000 PSI
3. BAR COVER = 1" U.N.C.
4. UNIT WEIGHT OF CON = 120 PCF
5. FILL DEPTH = 0.5' MIN. TO 15.0' MAX.
6. REINFORCEMENT SHALL BE BAR PER ASTM A615, GRADE 60 AND WWR PER ASTM A185/A497, GRADE 65.
7. REINFORCEMENT SHOWN (BAR) MUST BE REPLACED WITH WWR. MAINTAIN MIN. AREA OF STEEL = 0.37 IN<sup>2</sup>/FT AND MAXIMUM SPACING OF 10".



### REINFORCING SECTION NTS



### ISO VIEW (MULTIPLE CHAMBER CONFIGURATION) NTS



### CROSS SECTION TABLE REFERENCE

DESCRIPTION	STORMARC #XXX
A	MIN. DEPTH OF STONE BASE
B	CHAMBER HEIGHT
C	MIN. DEPTH REQUIRED OF 95% COMPACTED FILL FOR PAVED TRAFFIC
D	MIN. DEPTH OF COVER ALLOWED ABOVE CROWN OF STORM ARC
E	MAX. PIPE SIZE TO CHAMBER ENDWALL/ENDCAP

NOTES:  
 1: STONE BELOW UNITS MAY VARY PER SYSTEM. SEE SYSTEM LAYOUT FOR STONE REQUIREMENTS  
 2: CONSULT GOODFLOW SOLUTIONS FOR BURIAL DEPTHS OVER 8 FT.

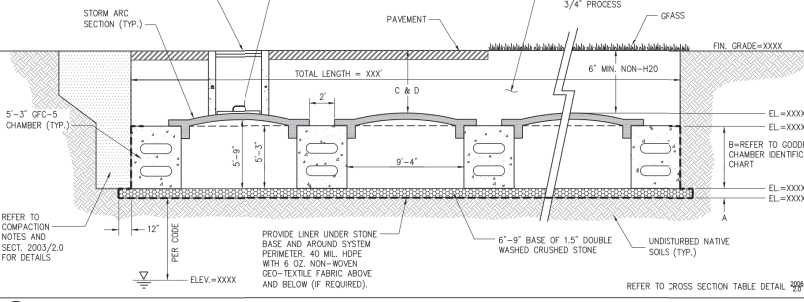
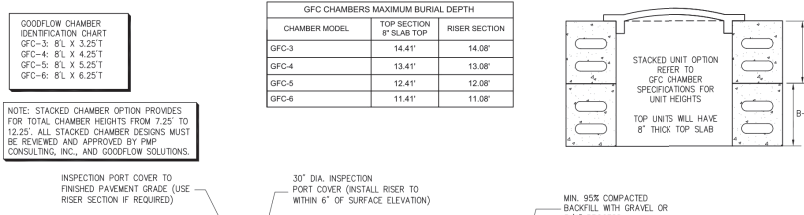
### STORAGE CHART CROSS-SECTION



### SYSTEM SIZING CALCULATIONS

TOTAL STORAGE VOLUME REQUIRED = XXXXXXXX C.F.  
 PROPOSED SYSTEM DESIGN:  
 XX ROWS OF X.25' TALL GFC CHAMBERS WITH X STORM ARC ROWS (SEE CROSS SECTION DETAIL)  
 OVERALL DIMENSIONS: XXXXX FT X XXX FT.  
 STORAGE AVAILABLE PER XX FT SECTION = XXXXXXXX C.F.  
 6'-0" BASE OF 1.5' DOUBLE WASHED CRUSHED STONE NOT INCLUDED IN STORAGE VOLUME  
 XXXXX C.F. / XXXXXX C.F. = XXXX X FT SECTIONS - USE XX SECTIONS  
 XX SECTIONS X XX FT = XXXX FT  
 TOTAL STORAGE PROVIDED: XXXXXX C.F. X XXX = XXXXXXXX C.F.  
 STONE BASE TO EXTEND 1 FT BEYOND CHAMBERS  
 TOTAL PERIMETER OF SYSTEM INCLUDING STONE BASE = XXXXXXXX FT.  
 TOTAL AREA OF SYSTEM INCLUDING STONE BASE = XXXXXXXX SF.  
 GOODFLOW MATERIALS REQUIRED:  
 X' GFC CHAMBERS: XX TOTAL  
 X CORNER UNITS  
 X END UNITS  
 XX SIDE UNITS  
 XXXX MIDDLE UNITS  
 STORM ARC SECTIONS: XXXX TOTAL

### STORM ARC BACKFILL SPECIFICATIONS CHARTS



### STORM ARC - END VIEW CROSS SECTION NTS

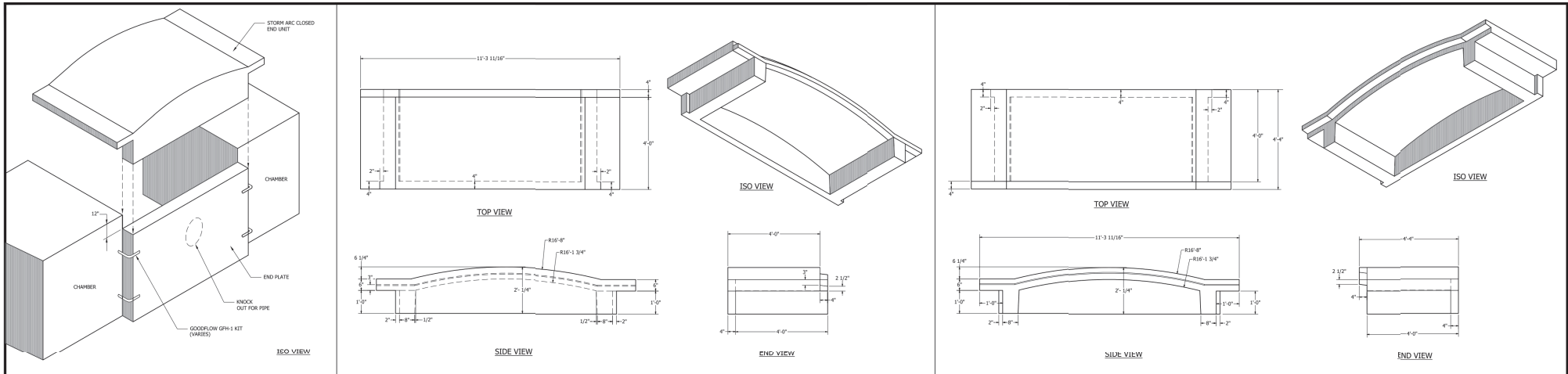
THIS DRAWING WAS PREPARED TO SUPPORT THE PROJECT ENGINEER OF RECORD FOR THE PROPOSED SYSTEM. IT IS THE ULTIMATE RESPONSIBILITY OF THE PROJECT ENGINEER OF RECORD TO ENSURE THAT THE GOODFLOW SYSTEMS DESIGN IS IN FULL COMPLIANCE WITH ALL APPLICABLE LAWS AND REGULATIONS. IT IS THE PROJECT ENGINEER OF RECORD'S RESPONSIBILITY TO ENSURE THAT THE GOODFLOW PRODUCTS ARE DESIGNED IN ACCORDANCE WITH GOODFLOW'S MINIMUM REQUIREMENTS. GOODFLOW DOES NOT APPROVE PLANS, SIZING, OR SYSTEM DESIGNS.

PROJECT\_NAME:  
 CITY\_STATE:  
 ADDRESS\_1:  
 ADDRESS\_2:

### PRECAST SOLID STORM ARC-001

PROJECT NO: XXXXXXXXXX	DATE: XX-XX-XXXX
DESIGNED BY: XXXXXXXXXX	CHECKED BY: XXXXXXXXXX
SCALE: AS NOTED	SHEET NO: 2 OF 4

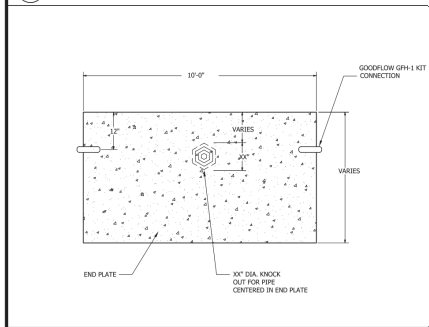
Manufacturer of Innovative Septic & Drainage Systems  
 23 Ryan Street  
 Stamford, CT 06907  
 www.GoodFlow.com  
 PH: (203) 869-2969  
 FX: (203) 869-2977  
 info@goodflow.com



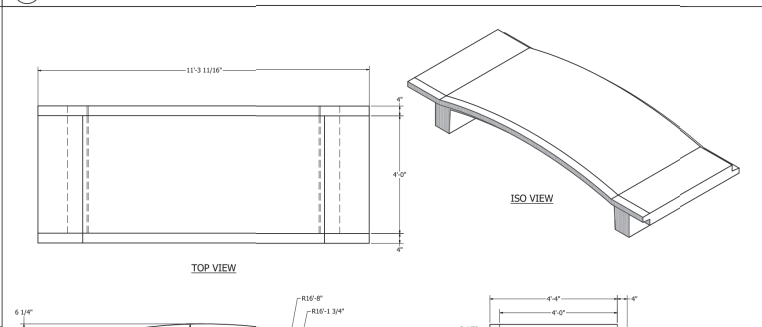
3004 3.0 STORM ARC - END PLATE NTS

3002 3.0 STORM ARC - END SECTION LEFT SIDE VIEW NTS

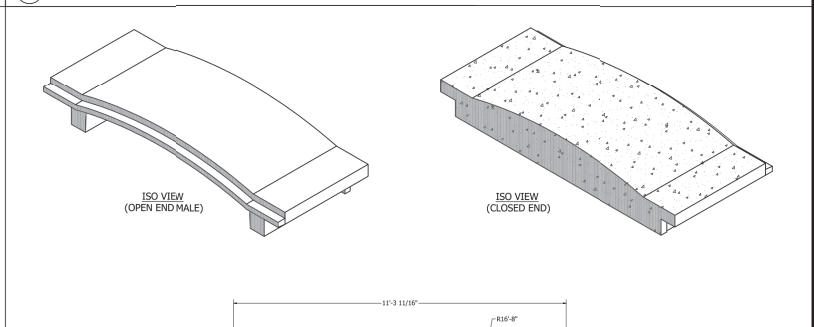
3001 3.0 STORM ARC - END SECTION RIGHT SIDE VIEW NTS



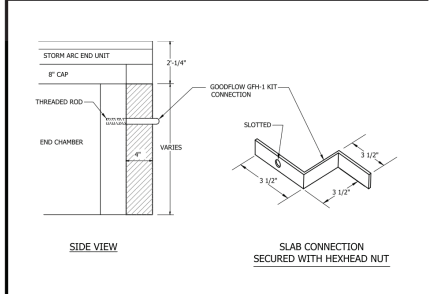
3004 3.0 STORM ARC - END PLATE FRONT VIEW NTS



3003 3.0 STORM ARC - MID SECTION SIDE VIEW NTS



3000 3.0 STORM ARC - END SECTIONS NTS



3004 3.0 STORM ARC - END PLATE DETAILS NTS

- CONCRETE SPECIFICATIONS**
1. CONCRETE MUST BE 5000 PSI AT 28 DAYS.
  2. MAXIMUM SIZE AGGREGATE LESS THAN 5/8\"/>
  3. REBAR IS TO BE TIED @ 10\"/>
  4. MINIMUM CONCRETE COVERAGE IS 1.50\"/>

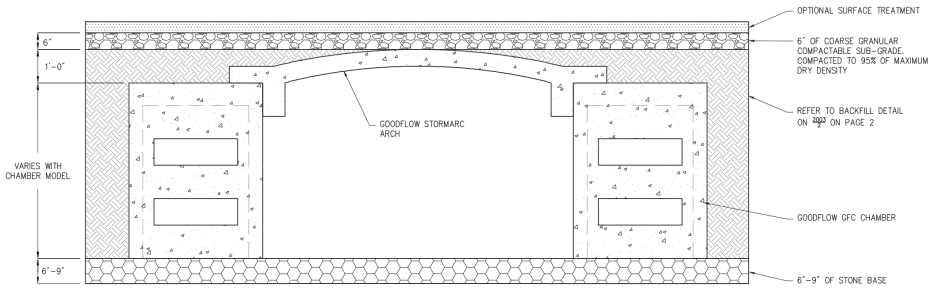
3000 3.0 STORM ARC - END SECTIONS NTS

**GOODFLOW SOLUTIONS**  
 Manufacturer of Innovative Septic & Drainage Systems  
 23 Ryan Street PH: (203) 869-2969  
 Stamford, CT 06907 F: (203) 869-2977  
 www.GoodFlow.com info@goodflow.com

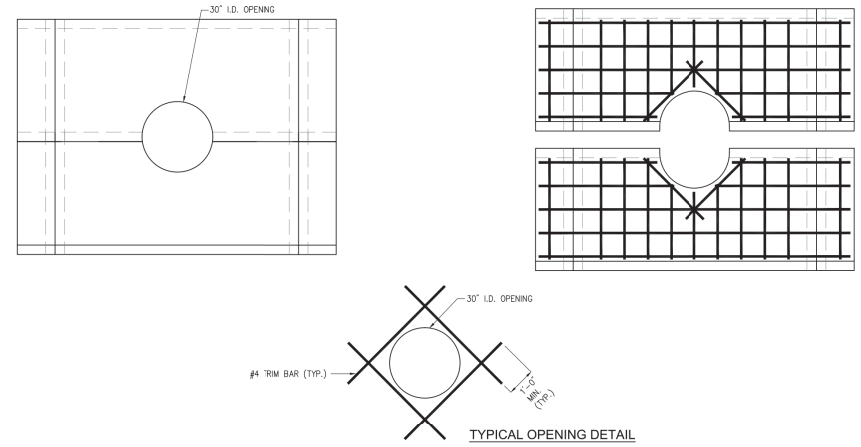
THIS DRAWING WAS PREPARED TO SUPPORT THE PROJECT ENGINEER OF RECORD FOR THE PROPOSED SYSTEM. IT IS THE ULTIMATE RESPONSIBILITY OF THE PROJECT ENGINEER OF RECORD TO ENSURE THAT THE GOODFLOW SYSTEMS DESIGN IS IN FULL COMPLIANCE WITH ALL APPLICABLE LAWS AND REGULATIONS. IT IS THE PROJECT ENGINEER OF RECORD'S RESPONSIBILITY TO ENSURE THAT THE GOODFLOW PRODUCTS ARE DESIGNED IN ACCORDANCE WITH GOODFLOW'S MINIMUM REQUIREMENTS. GOODFLOW DOES NOT APPROVE PLANS, SIZING, OR SYSTEM DESIGNS.

PROJECT\_NAME:  
 CITY\_STATE:  
 ADDRESS\_1:  
 ADDRESS\_2:

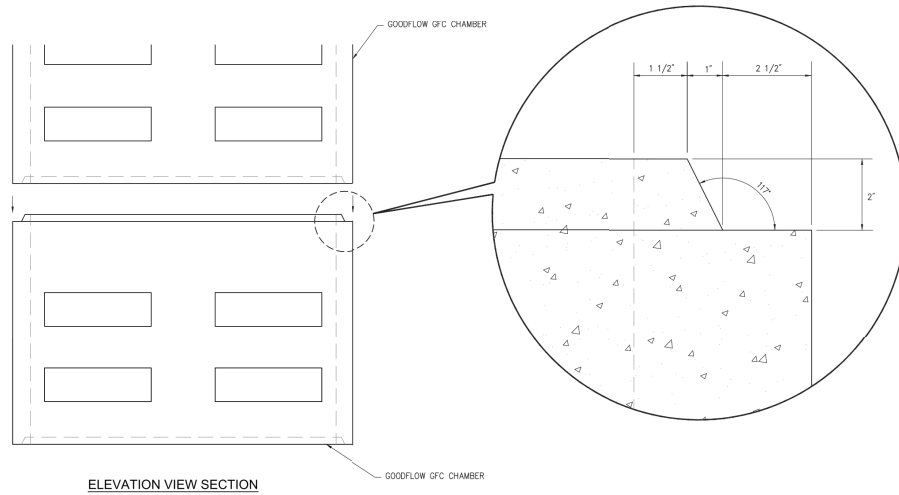
PRECAST SOLID STORM ARC-001	
PROJECT NO: XXXXXXXXX	DATE: XX-XX-XXXX
DESIGNED BY: XXXXXXXXX	CHECKED BY: XXXXXXXXX
SCALE: AS NOTED	SHEET NO: 3 OF 4



4000 4.0 STORM ARC - TYPICAL HS20 MINIMAL BACKFILL

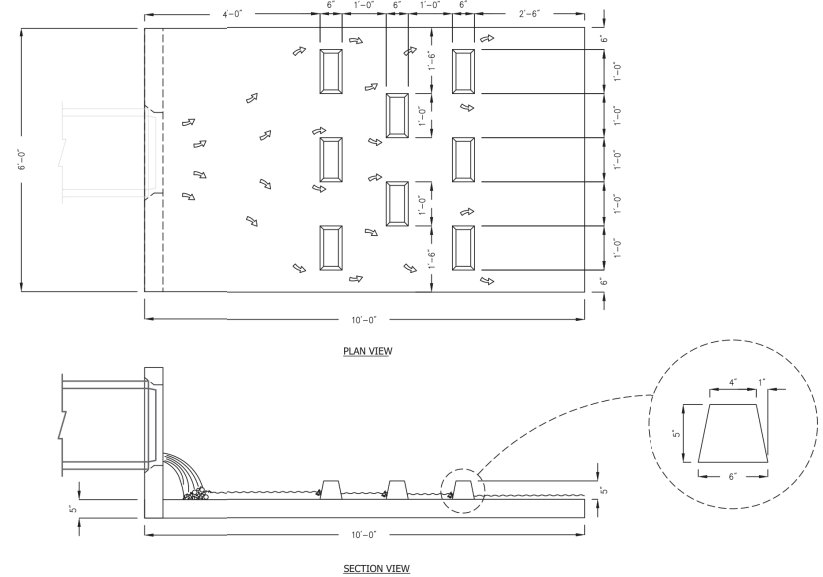


4000 4.0 STORM ARC - 30" I.D. OPENING



ELEVATION VIEW SECTION

4000 4.0 STORM ARC - STACKABLE KEYWAY SECTION



PLAN VIEW

SECTION VIEW

4000 4.0 STORM ARC - SCOUR CONCRETE PAD

4000 4.0

NTS

4000 4.0

NTS

4000 4.0

NTS

4000 4.0

NTS

GOODFLOW SOLUTIONS  
 Manufacturer of Innovative Septic & Drainage Systems  
 23 Ryan Street PH: (203) 869-2969  
 Stamford, CT 06907 F: (203) 869-2977  
 www.GoodFlow.com info@goodflow.com

THIS DRAWING WAS PREPARED TO SUPPORT THE PROJECT ENGINEER OF RECORD FOR THE PROPOSED SYSTEM. IT IS THE ULTIMATE RESPONSIBILITY OF THE PROJECT ENGINEER OF RECORD TO ENSURE THAT THE GOODFLOW SYSTEMS DESIGN IS IN FULL COMPLIANCE WITH ALL APPLICABLE LAWS AND REGULATIONS. IT IS THE PROJECT ENGINEER OF RECORD'S RESPONSIBILITY TO ENSURE THAT THE GOODFLOW PRODUCTS ARE DESIGNED IN ACCORDANCE WITH GOODFLOW'S MINIMUM REQUIREMENTS. GOODFLOW DOES NOT APPROVE PLANS, SIZING, OR SYSTEM DESIGNS.

PROJECT\_NAME:  
 CITY\_STATE:  
 ADDRESS\_1:  
 ADDRESS\_2:

PRECAST SOLID STORM ARC-001

PROJECT NO: XXXXXXXXX	DATE: XX-XX-XXXX
DESIGNED BY: XXXXXXXXX	CHECKED BY: XXXXXXXXX
SCALE: AS NOTED	SHEET NO: 4 OF 4

**Location Map**



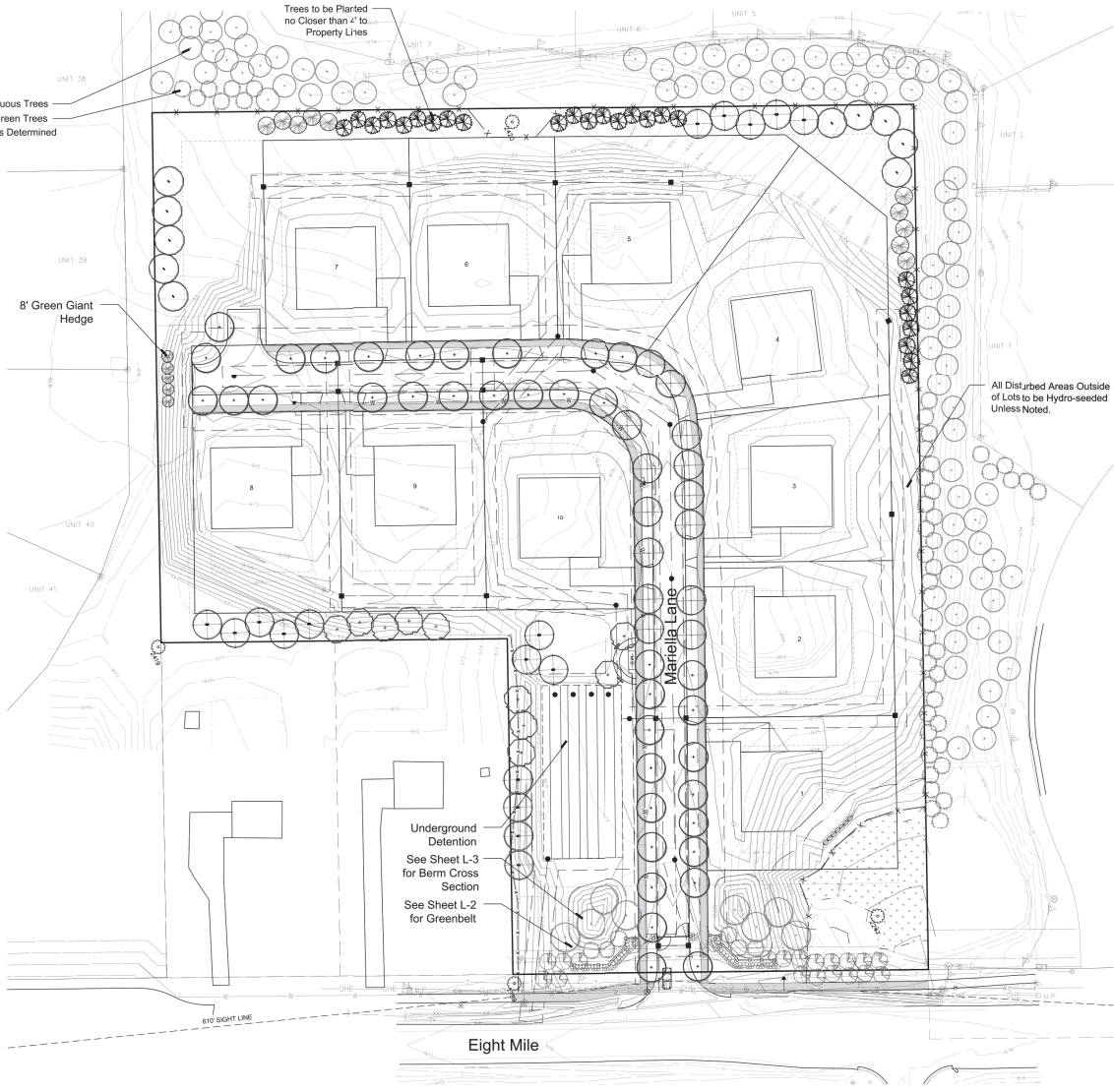
**Bench**



**Landscape Summary**

Street Trees	1,788 l.f.
Street Frontage	51 Trees (1,788 / 35)
Trees Required	51 Trees
Trees Provided	51 Trees
Woodland Replacement	10 Trees
Trees Required	10 Trees
Trees Provided	10 Trees
Sheet L-1	10 Trees

- Notes:
1. Soils Information is Found on Sheet 2.
  2. Trees Shall be Planted no Closer than 10' Driveways, Sanitary Sewer, Utility Structures Including Hydrants and 5' from Utility Lines.
  3. Trees Shall not be Planted within 4' of Property Lines.
  4. Utility Boxes Shall be Screen per Detail on Sheet L-3.
  5. Phragmites and Japanese Knotweed are Not Present on this Site.
  6. Overhead Power Lines are Indicated on the Plan Along 8 Mile.
  7. An Irrigation Plan will be Provided for Stamping Sets.
  8. Snow Deposit will be Along the Streets Between the Curb and Sidewalk.



Seal:



Title:  
**Landscape Plan**

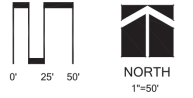
Project:  
**Mariella Estates  
 Novi, Michigan**

Prepared for:  
 Braceio Brothers, LLC  
 48355 Anna Court  
 Wixom, Michigan 48393  
 Antonello Stante, 248.380.9922

<b>Revision:</b>	<b>Issued:</b>
Submission	May 1, 2024
Revised	November 6, 2024
Revised	February 28, 2025
Revised	June 27, 2025

Job Number:  
 24-023

Drawn By: jca  
 Checked By: jca

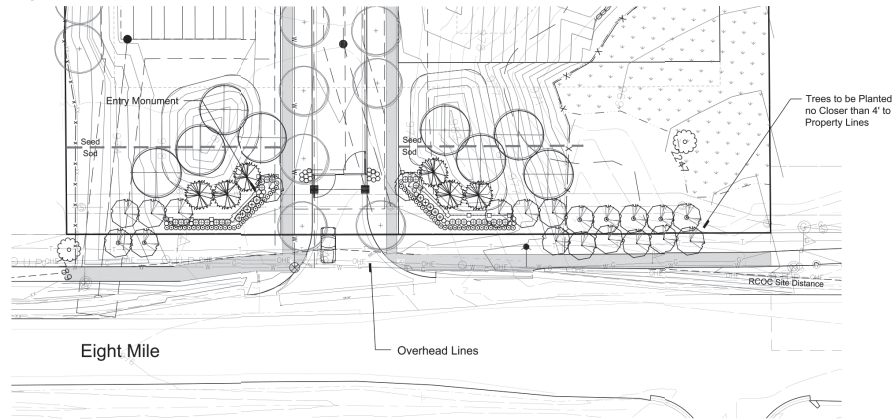


Sheet No.

L-1



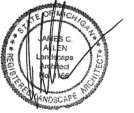
**Eight Mile Greenbelt**



**Landscape Summary**

<b>Greenbelt Plantings</b>	
<b>Eight Mile</b>	
Total Street Frontage	360 l.f.
Less ROW	28 l.f.
Net Frontage	332 l.f.
Canopy Trees Required	8.3 Trees (332 / 40)
Canopy Trees Provided	8 Trees
Sub-Canopy Trees Required	13.3 Trees (332 / 25)
Sub-Canopy Trees Provided	13 Trees
<b>Street Trees</b>	
<b>Eight Mile</b>	
Total Street Frontage	360 l.f.
Less RCOC Site Vision	294 l.f.
Net Frontage	66 l.f.
Trees Required	1.9 Trees (66 / 35)
Ornamental Trees Provided	5 Trees (1.5 Ornamental per Tree)
Note:	
1. Trees Shall be Planted no Closer than 10' Driveways, Sanitary Sewer, Utility Structures Including Hydrants and 5' from Utility Lines.	
2. Trees Shall not be Planted within 4' of Property Lines.	

Seal:



Title:

**Greenbelt and Detention**

Project:

**Mariella Estates  
 Novi, Michigan**

Prepared for:

**Bracole Brothers, LLC  
 48355 Anna Court  
 Wixom, Michigan 48393  
 Antonello Stante, 248.380.9922**

Revision: Issued:

Submission	May 1, 2024
Revised	November 6, 2024
Revised	February 28, 2025
Revised	June 27, 2027

Job Number:

24-403

Drawn By: Checked By:

jca jca

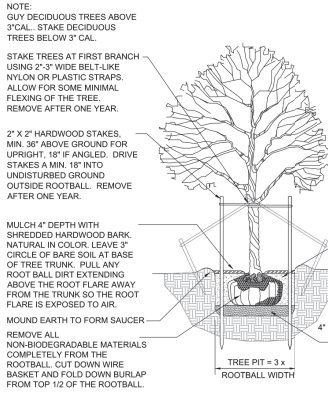


Sheet No.

**L-2**

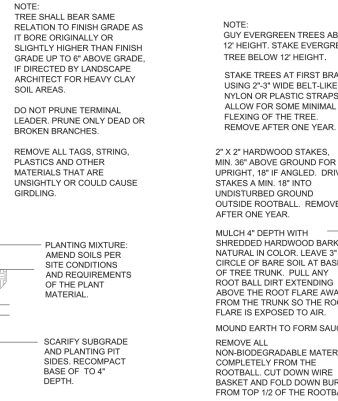






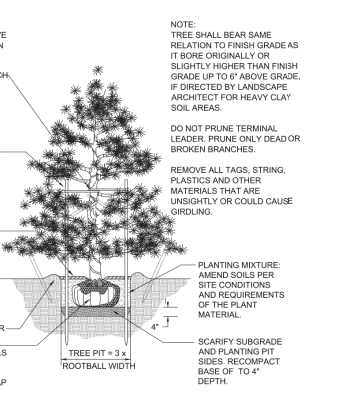
**DECIDUOUS TREE PLANTING DETAIL**

Not to scale



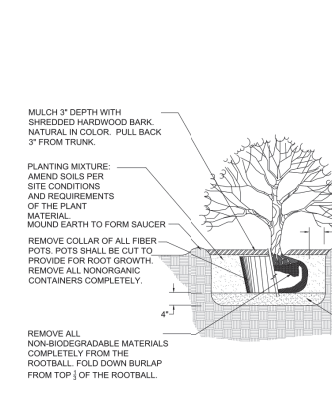
**EVERGREEN TREE PLANTING DETAIL**

Not to scale

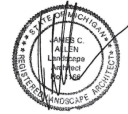


**SHRUB PLANTING DETAIL**

NOT TO SCALE



Seal:



Title:

Landscape Details

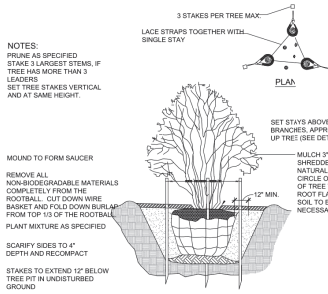
Project:

Mariella Estates  
Novi, Michigan

Prepared for:

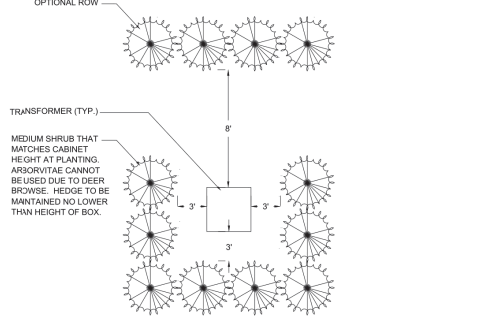
Bracole Brothers, LLC  
48555 Anna Court  
Wixom, Michigan 48393  
Antonio Stante, 248.380.9922

Revision:	Issued:
Submission	May 1, 2024
Revised	November 6, 2024
Revised	February 28, 2025
Revised	June 27, 2025



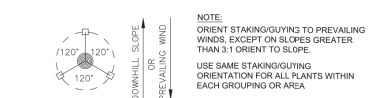
**MULTI-STEM TREE PLANTING DETAIL**

NOT TO SCALE

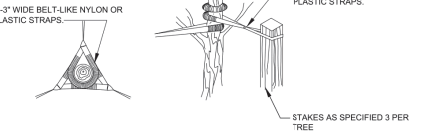


**TRANSFORMER SCREENING DETAIL**

Not to scale



**STAKING/GUYING LOCATION**



**GUYING DETAIL**

**STAKING DETAIL**

**TREE STAKING DETAIL**

Not to scale



**PERENNIAL PLANTING DETAIL**

Not to scale

**Berm Detail - 8 Mile**

HORIZONTAL SCALE: 1"=10'

**LANDSCAPE NOTES**

- All plants shall be north Midwest American region grown, No. 1 grade plant materials, and shall be true to name, free from physical damage and wind burn.
- Plants shall be full, well-branched, and in healthy vigorous growing condition.
- Plants shall be watered before and after planting is complete.
- All trees must be staked, fertilized and mulched and shall be guaranteed to exhibit a normal growth cycle for at least two (2) full years following City approval.
- All material shall conform to the guidelines established in the most recent edition of the American Standard for Nursery Stock.
- Provide clean backfill soil, using material stockpiled on site. Soil shall be screened and free of any debris, foreign material, and stone.
- \*Agriflora\* tabs or similar slow-release fertilizer shall be added to the planting pits before backfill.
- Amended planting mix shall consist of 1/3 screened topsoil, 1/3 sand and 1/3 compost, mixed well and spread to the depth as indicated in planting details.
- All plantings shall be mulched per planting details located on this sheet.
- The Landscape Contractor shall be responsible for all work shown on the landscape drawings and specifications.
- No substitutions or changes of location, or plant types shall be made without the approval of the Landscape Architect.
- The Landscape Architect shall be notified in writing of any discrepancies between the plans and field conditions prior to installation.
- The Landscape Contractor shall be responsible for maintaining all plant material in a vertical condition throughout the guaranteed period.
- The Landscape Architect shall have the right, at any stage of the installation, to reject any work or material that does not meet the requirements of the plans and specifications, if requested by owner.
- Contractor shall be responsible for checking plant quantities to ensure quantities on drawings and plant list are the same. In the event of a discrepancy, the quantities on the plans shall prevail.
- The Landscape Contractor shall seed and mulch or sod (as indicated on plans) all areas disturbed during construction, throughout the contract limits.
- A pre-emergent weed control agent, "Preem" or equal, shall be applied uniformly on top of all mulching in all planting beds.
- Sod shall be two year old "Baron/Cheriedeph" Kentucky Blue Grass grown in a sod nursery on loam soil.

**CITY OF NOVI NOTES**

- All landscape islands shall be backfilled with a sand mixture to facilitate drainage.
- All proposed landscape islands shall be curbed.
- All landscape areas shall be irrigated.
- Overhead utility lines and poles to be relocated as directed by utility company of record.
- Evergreen and canopy trees shall be planted a minimum of 10' from a fire hydrant, and manhole, 15' from overhead wires.
- All plant material shall be guaranteed for two (2) years after City Approval and shall be installed and maintained according to City of Novi standards. Replace Failing Material within 3 Months of Discovering the Need for Replacement. One cultivation per month shall occur in June-August.
- All proposed street trees shall be planted a minimum of 4' from both the back of curb and proposed walks.
- All tree and shrub planting beds shall be mulched with shredded hardwood bark, spread to minimum depth of 4". All lawn area trees shall have a 4" diameter circle of shredded hardwood mulch 3" away from trunk. All perennial, annual and ground cover beds shall receive 2" of bark colored bark mulch as indicated on the plant list. Mulch is to be free from debris and foreign material, and shall contain no pieces of inconsistent size.
- All Substitutions or Deviations from the Landscape Plan Must be Approved in Writing by the City of Novi Prior to their Installation.

**NOTES:**  
THE APPROXIMATE DATE OF INSTALLATION FOR THE PROPOSED LANDSCAPE WILL BE MARCH 15 AND NOVEMBER 15.  
THE SITE WILL BE MAINTAINED BY THE DEVELOPER IN ACCORDANCE WITH THE STANDARDS SET FORTH IN THE CITY OF NOVI ZONING ORDINANCE. THIS INCLUDES WEEDING AND WATERING AS REQUIRED BY NORMAL MAINTENANCE PRACTICES.  
DEVELOPER SHALL BE RESPONSIBLE FOR REPLACING ANY TREES WITHIN UTILITY EASEMENTS THAT ARE DAMAGED THROUGH NORMAL MAINTENANCE OR REPAIRS.  
PLANT MATERIALS SHALL BE GUARANTEED FOR 2 YEARS AND SHALL BE MAINTAINED IN ACCORDANCE WITH CITY ORDINANCES. WARRANTY PERIOD BEGINS AT THE TIME OF CITY APPROVAL. WATERING AS NECESSARY SHALL OCCUR DURING THIS WARRANTY PERIOD.  
ANY SUBSTITUTIONS MUST BE SUBMITTED IN WRITING AND APPROVED BY THE CITY.



Sheet No.

L-3

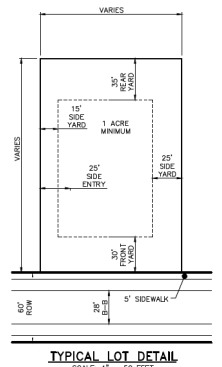
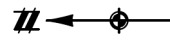
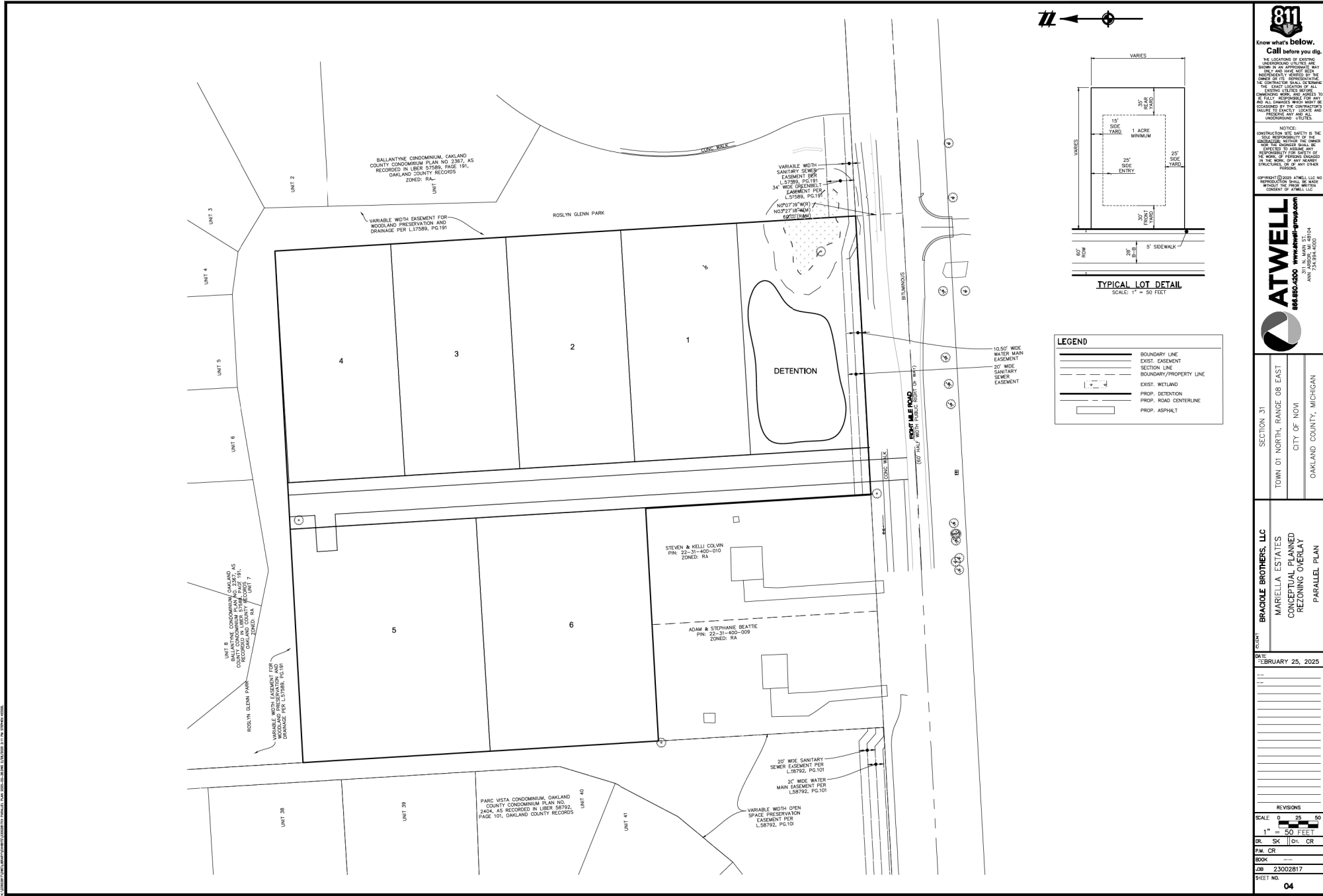


**Tree List**

Tree Tag #	Scientific Name	Common Name	DBH (in/ft)	Condition	Comments	To Be Removed	Replacements	Credits
1765	Acer glaberrimum	Norway Spruce	26	Good		Yes		
1766	Pinus strobus	White Pine	18	Fair	Dead Branches	Yes		
1767	Acer saccharum	Sugar Maple	22	Fair	Fused Trunk	Yes	4	
1768	Acer saccharum	Sugar Maple	22	Fair	Trapped for Utility Line	Yes		
1769	Acer saccharum	Sugar Maple	23	Fair	Trapped for Utility Line	Yes		
1770	Malus spectata	Apple/Crabapple	15	Fair	Two Trunk (T1), 32, Dead Main Branches	Yes		
1771	Acer saccharum	Sugar Maple	43.5	Good	T1, 26, Fused Trunk	Yes	4	
1772	Liriodendron tulipifera	Eastern Red Cedar	14.5	Good	Dead Branches, Missing Bark	Yes		
1773	Magnolia grandiflora	Magnolia	15	Good	Three Trunk (T1), 32, 8	Yes		
1774	Acer saccharum	Silver Maple	39.5	Good		Yes		
1775	Acer saccharum	Sugar Maple	21	Good		Yes		
1776	Acer saccharum	Silver Maple	20	Good		Yes		
1777	Acer saccharum	Silver Maple	24	Good		Yes		
1778	Acer saccharum	Silver Maple	25	Good		Yes		
1779	Acer saccharum	Silver Maple	32	Good	Seven Trunk (T1), 13, 13, 8.5, 6.5, 6, 5.5	Yes		
2217	Arundinaria gigantea	Ohio River Cane	82.5	Fair	Removed Main Limb	Yes		
2218	Magnolia grandiflora	Magnolia	17.5	Fair	T1, 14.5, Missing Branches	Yes	4	
2219	Acer saccharum	Silver Maple	35	Good		Yes		
2220	Acer saccharum	Silver Maple	27.5	Good		Yes		
2221	Acer saccharum	Silver Maple	35	Fair	Dead Limbs, Trimmings	No		
2222	Acer saccharum	Silver Maple	40.5	Good		Yes	4	
2223	Acer saccharum	Silver Maple	40.5	Good		Yes		
2224	Acer saccharum	Silver Maple	35	Good	T1, 13, 11	Yes		
2225	Acer saccharum	Silver Maple	43.5	Good		Yes		
2226	Acer saccharum	Silver Maple	40.5	Good		Yes		
2227	Acer saccharum	Silver Maple	40.5	Good		Yes		
2228	Acer saccharum	Silver Maple	40.5	Good		Yes		
2229	Acer saccharum	Silver Maple	40.5	Good		Yes		
2230	Acer saccharum	Silver Maple	40.5	Good		Yes		
2231	Acer saccharum	Silver Maple	40.5	Good		Yes		
2232	Acer saccharum	Silver Maple	40.5	Good		Yes		
2233	Acer saccharum	Silver Maple	40.5	Good		Yes		
2234	Acer saccharum	Silver Maple	40.5	Good		Yes		
2235	Acer saccharum	Silver Maple	40.5	Good		Yes		
2236	Acer saccharum	Silver Maple	40.5	Good		Yes		
2237	Acer saccharum	Silver Maple	40.5	Good		Yes		
2238	Acer saccharum	Silver Maple	40.5	Good		Yes		
2239	Acer saccharum	Silver Maple	40.5	Good		Yes		
2240	Acer saccharum	Silver Maple	40.5	Good		Yes		
2241	Acer saccharum	Silver Maple	40.5	Good		Yes		
2242	Acer saccharum	Silver Maple	40.5	Good		Yes		
2243	Acer saccharum	Silver Maple	40.5	Good		Yes		
2244	Acer saccharum	Silver Maple	40.5	Good		Yes		
2245	Acer saccharum	Silver Maple	40.5	Good		Yes		
2246	Acer saccharum	Silver Maple	40.5	Good		Yes		
2247	Acer saccharum	Silver Maple	40.5	Good		Yes		
2248	Acer saccharum	Silver Maple	40.5	Good		Yes		
2249	Acer saccharum	Silver Maple	40.5	Good		Yes		
2250	Acer saccharum	Silver Maple	40.5	Good		Yes		
2251	Acer saccharum	Silver Maple	40.5	Good		Yes		
2252	Acer saccharum	Silver Maple	40.5	Good		Yes		
2253	Acer saccharum	Silver Maple	40.5	Good		Yes		
2254	Acer saccharum	Silver Maple	40.5	Good		Yes		
2255	Acer saccharum	Silver Maple	40.5	Good		Yes		
2256	Acer saccharum	Silver Maple	40.5	Good		Yes		
2257	Acer saccharum	Silver Maple	40.5	Good		Yes		
2258	Acer saccharum	Silver Maple	40.5	Good		Yes		
2259	Acer saccharum	Silver Maple	40.5	Good		Yes		
2260	Acer saccharum	Silver Maple	40.5	Good		Yes		
2261	Acer saccharum	Silver Maple	40.5	Good		Yes		
2262	Acer saccharum	Silver Maple	40.5	Good		Yes		
2263	Acer saccharum	Silver Maple	40.5	Good		Yes		
2264	Acer saccharum	Silver Maple	40.5	Good		Yes		
2265	Acer saccharum	Silver Maple	40.5	Good		Yes		
2266	Acer saccharum	Silver Maple	40.5	Good		Yes		
2267	Acer saccharum	Silver Maple	40.5	Good		Yes		
2268	Acer saccharum	Silver Maple	40.5	Good		Yes		
2269	Acer saccharum	Silver Maple	40.5	Good		Yes		
2270	Acer saccharum	Silver Maple	40.5	Good		Yes		
2271	Acer saccharum	Silver Maple	40.5	Good		Yes		
2272	Acer saccharum	Silver Maple	40.5	Good		Yes		
2273	Acer saccharum	Silver Maple	40.5	Good		Yes		
2274	Acer saccharum	Silver Maple	40.5	Good		Yes		
2275	Acer saccharum	Silver Maple	40.5	Good		Yes		
2276	Acer saccharum	Silver Maple	40.5	Good		Yes		
2277	Acer saccharum	Silver Maple	40.5	Good		Yes		
2278	Acer saccharum	Silver Maple	40.5	Good		Yes		
2279	Acer saccharum	Silver Maple	40.5	Good		Yes		
2280	Acer saccharum	Silver Maple	40.5	Good		Yes		
2281	Acer saccharum	Silver Maple	40.5	Good		Yes		
2282	Acer saccharum	Silver Maple	40.5	Good		Yes		
2283	Acer saccharum	Silver Maple	40.5	Good		Yes		
2284	Acer saccharum	Silver Maple	40.5	Good		Yes		
2285	Acer saccharum	Silver Maple	40.5	Good		Yes		
2286	Acer saccharum	Silver Maple	40.5	Good		Yes		
2287	Acer saccharum	Silver Maple	40.5	Good		Yes		
2288	Acer saccharum	Silver Maple	40.5	Good		Yes		
2289	Acer saccharum	Silver Maple	40.5	Good		Yes		
2290	Acer saccharum	Silver Maple	40.5	Good		Yes		
2291	Acer saccharum	Silver Maple	40.5	Good		Yes		
2292	Acer saccharum	Silver Maple	40.5	Good		Yes		
2293	Acer saccharum	Silver Maple	40.5	Good		Yes		
2294	Acer saccharum	Silver Maple	40.5	Good		Yes		
2295	Acer saccharum	Silver Maple	40.5	Good		Yes		
2296	Acer saccharum	Silver Maple	40.5	Good		Yes		
2297	Acer saccharum	Silver Maple	40.5	Good		Yes		
2298	Acer saccharum	Silver Maple	40.5	Good		Yes		
2299	Acer saccharum	Silver Maple	40.5	Good		Yes		
2300	Acer saccharum	Silver Maple	40.5	Good		Yes		
2301	Acer saccharum	Silver Maple	40.5	Good		Yes		
2302	Acer saccharum	Silver Maple	40.5	Good		Yes		
2303	Acer saccharum	Silver Maple	40.5	Good		Yes		
2304	Acer saccharum	Silver Maple	40.5	Good		Yes		
2305	Acer saccharum	Silver Maple	40.5	Good		Yes		
2306	Acer saccharum	Silver Maple	40.5	Good		Yes		
2307	Acer saccharum	Silver Maple	40.5	Good		Yes		
2308	Acer saccharum	Silver Maple	40.5	Good		Yes		
2309	Acer saccharum	Silver Maple	40.5	Good		Yes		
2310	Acer saccharum	Silver Maple	40.5	Good		Yes		
2311	Acer saccharum	Silver Maple	40.5	Good		Yes		
2312	Acer saccharum	Silver Maple	40.5	Good		Yes		
2313	Acer saccharum	Silver Maple	40.5	Good		Yes		
2314	Acer saccharum	Silver Maple	40.5	Good		Yes		
2315	Acer saccharum	Silver Maple	40.5	Good		Yes		
2316	Acer saccharum	Silver Maple	40.5	Good		Yes		
2317	Acer saccharum	Silver Maple	40.5	Good		Yes		
2318	Acer saccharum	Silver Maple	40.5	Good		Yes		
2319	Acer saccharum	Silver Maple	40.5	Good		Yes		
2320	Acer saccharum	Silver Maple	40.5	Good		Yes		
2321	Acer saccharum	Silver Maple	40.5	Good		Yes		
2322	Acer saccharum	Silver Maple	40.5	Good		Yes		
2323	Acer saccharum	Silver Maple	40.5	Good		Yes		
2324	Acer saccharum	Silver Maple	40.5	Good		Yes		
2325	Acer saccharum	Silver Maple	40.5	Good		Yes		
2326	Acer saccharum	Silver Maple	40.5	Good		Yes		
2327	Acer saccharum	Silver Maple	40.5	Good		Yes		
2328	Acer saccharum	Silver Maple	40.5	Good		Yes		
2329	Acer saccharum	Silver Maple	40.5	Good		Yes		
2330	Acer saccharum	Silver Maple	40.5	Good		Yes		
2331	Acer saccharum	Silver Maple	40.5	Good		Yes		
2332	Acer saccharum	Silver Maple	40.5	Good		Yes		
2333	Acer saccharum	Silver Maple	40.5	Good		Yes		
2334	Acer saccharum	Silver Maple	40.5	Good		Yes		
2335	Acer saccharum	Silver Maple	40.5	Good		Yes		
2336	Acer saccharum	Silver Maple	40.5	Good		Yes		
2337	Acer saccharum	Silver Maple	40.5	Good		Yes		
2338	Acer saccharum	Silver Maple	40.5	Good		Yes		
2339	Acer saccharum	Silver Maple	40.5	Good		Yes		
2340	Acer saccharum	Silver Maple	40.5	Good		Yes		
2341	Acer saccharum	Silver Maple	40.5	Good		Yes		
2342	Acer saccharum	Silver Maple	40.5	Good		Yes		
2343	Acer saccharum	Silver Maple	40.5	Good		Yes		
2344	Acer saccharum	Silver Maple	40.5	Good		Yes		
2345	Acer saccharum	Silver Maple	40.5	Good		Yes		
2346	Acer saccharum	Silver Maple	40.5	Good		Yes		
2347	Acer saccharum	Silver Maple	40.5	Good		Yes		
2348	Acer saccharum	Silver Maple	40.5	Good		Yes		
2349	Acer saccharum	Silver Maple	40.5	Good		Yes		
2350	Acer saccharum	Silver Maple	40.5	Good		Yes		
2351	Acer saccharum	Silver Maple	40.5	Good		Yes		
2352	Acer saccharum	Silver Maple	40.5	Good		Yes		
2353	Acer saccharum	Silver Maple	40.5	Good		Yes		
2354	Acer saccharum	Silver Maple	40.5	Good		Yes		
2355	Acer saccharum	Silver Maple	40.5	Good		Yes		
2356	Acer saccharum	Silver Maple	40.5	Good		Yes		
2357	Acer saccharum	Silver Maple	40.5	Good		Yes		
2358	Acer saccharum	Silver Maple	40.5	Good		Yes		
2359	Acer saccharum	Silver Maple	40.5	Good		Yes		
2360	Acer saccharum	Silver Maple	40.5	Good		Yes		
2361	Acer saccharum	Silver Maple	40.5	Good		Yes		
2362	Acer saccharum	Silver Maple	40.5</					

## PARALLEL PLAN

---



**LEGEND**

	BOUNDARY LINE
	EXIST. EASEMENT
	SECTION LINE
	BOUNDARY/PROPERTY LINE
	EXIST. WETLAND
	PROP. DETENTION
	PROP. ROAD CENTERLINE
	PROP. ASPHALT

**Know what's below.**  
Call before you dig.

THE LOCATION OF EXISTING UNDERGROUND UTILITIES MAY ONLY BE DETERMINED BY THE CONTRACTOR'S USE OF THE SERVICES PROVIDED BY THE CONTRACTOR'S QUALIFIED PERSONNEL. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE LOCATION OF ALL UTILITIES. THE CONTRACTOR SHALL BE FULLY RESPONSIBLE FOR ANY DAMAGE TO EXISTING UTILITIES. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION AND PRESERVATION OF ALL UTILITIES. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION AND PRESERVATION OF ALL UTILITIES.

**NOTICE:** CONSTRUCTION SITE SAFETY IS THE RESPONSIBILITY OF THE CONTRACTOR. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION AND PRESERVATION OF ALL UTILITIES. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION AND PRESERVATION OF ALL UTILITIES.

ATWELL  
888.850.4000 www.atwell-group.com  
ANN ARBOR, MI 48104  
734.954.4000

<b>CLIENT</b>	BRACIOLE BROTHERS, LLC
	MARIELLA ESTATES CONCEPTUAL PLANNED REZONING OVERLAY PARALLEL PLAN
<b>SECTION</b>	SECTION 31
<b>TOWN</b>	TOWN 01 NORTH, RANGE 08 EAST
<b>CITY</b>	CITY OF NOVI
<b>COUNTY</b>	OAKLAND COUNTY, MICHIGAN
<b>DATE</b>	FEBRUARY 25, 2025
<b>REVISIONS</b>	
<b>SCALE</b>	1" = 50 FEET
<b>DESIGNER</b>	SK CH CR
<b>CHECKER</b>	CR
<b>BOOK</b>	
<b>JOB</b>	23002817
<b>SHEET NO.</b>	04

## PROJECT NARRATIVE

---



August 14, 2025

Ms. Lindsay Bell  
City of Novi – Planning Department  
45175 West Ten Mile Road  
Novi, Michigan 48375

Re: Mariella Estates  
Planned Rezoning Overlay (PRO) Submittal Package  
JZ24-43

Dear Ms. Bell,

We are pleased to present to you a proposed single family residential development by Braciolo Brothers LLC. Please accept this letter document, accompanying plans, and supplemental information as the Conceptual PRO submittal for our client's Preserves of Maybury development. We are providing these plans for your distribution and comments.

### **PROJECT OVERVIEW**

The Preserves at Maybury project is a single-family residential community proposed on approximately 9-acres in the City of Novi, Oakland County, Michigan. The proposed development is located on the north side of 8-Mile Road, just west of Garfield Road. The subject property is directly adjacent and surrounded by two RUD developments on either side, the Ballantyne and Parc Vista. The subject parcel and surrounding parcels in the area are currently zoned RA and prominently support development of 1/2-acre lots. The development will utilize the City's Planned Rezoning Overlay (PRO) option to allow for single family housing at a lot size and density more in line with that of the directly adjacent RUD developments. Multiple public benefits are being proposed as a part of the PRO that are at the scale appropriate for a residential development with 10 houses being proposed.

The development will contain private roads and is proposed to be served by public sewer and water located within the 8-mile road right-of-way. These public utilities will have the capacity to serve the development per the city's engineering standards. Storm water management is proposed to be addressed through the construction of an underground detention vault on the west side of the property. The vault will be designed in accordance with the city's requirements.

The development is planned to be constructed in one phase.

### **PARALLEL PLAN**

Under the current RA zoning a maximum of 9 lots would be allowed based on site acreage. However, given geometric challenges of the property as well as providing an access road and stormwater detention, 6 lots are represented on the enclosed parallel plan.

## **PUBLIC BENEFITS FOR REZONING**

The requested R-1 underlay zoning, with a PRO Development Agreement would be the most cohesive option for development of this site to maintain a consistent lot size and product type between the two adjacent developments. Additionally, R-1 zoning is in compliance with the “Single-Family” designation listed for this site in the City of Novi Master Plan. As required with all PRO requests, we offer the following specific public benefits with the PRO project:

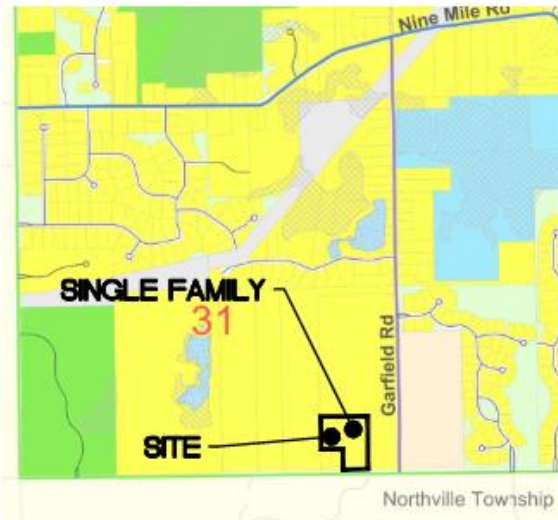
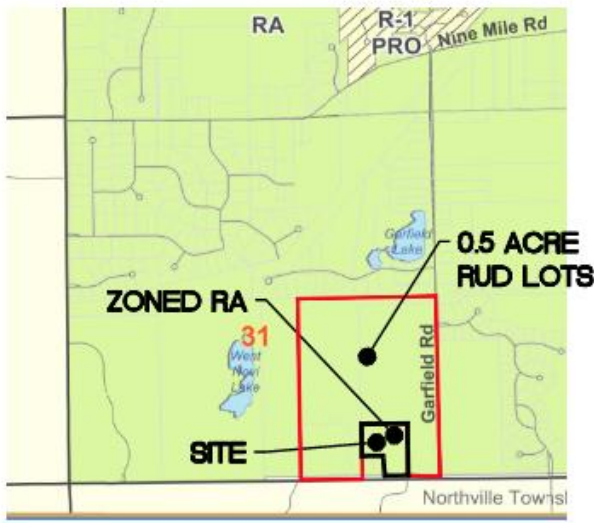
- ITC Park Donation: As part of the development the developer proposes a donation of \$40,000 to be used on improvements to the nearby ITC park. Details regarding specific improvements can be found in the letter included with the submittal.
- Perimeter Landscape Buffers: The development proposes a minimum 25' perimeter landscape buffer from the lots to the property boundary.
- Increased Open Space: The development proposes an extensive amount of open space for a single-family development (28%) and a majority of the proposed open space is usable active open space.
- Reduced Density: A reduced density of 1.07 units per acre is being proposed. R-1 zoning allows up to 1.65 units per acre.
- Conservation Easement over the Existing Wetland: The applicant will provide a conservation easement over the existing wetland and wetland buffer.

## **REQUESTED ORDINANCE DEVIATIONS**

Three deviations are being requested, as follows:

- Lots widths for lots 4 and 5 are 98' wide. This is 22' below the 120' minimum lot width required under R-1 zoning as measured at the front setback line. These lots still provide adequate space for the intended housing product, are the two largest lots proposed, and exceed the R-1 minimum lot area requirements for over 10,000 square feet and 5,000 square feet respectively.
- No cul-de-sac is being provided. Given the low volume of traffic that this subdivision will encounter a T-turnaround is being proposed due to geometric constraints and a way to reduce total pavement on site. The dimensions of the proposed turnaround meets current International Fire Code (IFC) requirements.
- A reduced centerline radius of 90 degrees is proposed due to geometric site constrains. This does not provide a safety concern given the short distance of the road, the low travel speed and the minimal traffic volumes expected.

**ZONING AND FUTURE LAND USE MAPS (FOR REFERENCE)**



**CONCLUSION**

We look forward to hearing the Planning Commissions feedback and hopeful recommendation for approval. We appreciate your continued assistance and cooperation with respect to this project. If you should have any questions or need any additional information, please contact us.

Sincerely,  
**ATWELL, LLC**

Chris Rothhaar, P.E.  
Project Manager



**PUBLIC BENEFITS DECLARATION LETTER**

---



July 11, 2025

Novi City Council  
45175 Ten Mile Road  
Novi, MI 48375

RE: JZ24-43 MARIELLA ESTATES PRO

Dear Members of Council, Planning Commission, and City Staff,

In response to feedback from the Planning Commission, City Council, and City Staff, we've worked with the applicant to identify public benefits that we can offer as part of the PRO that are both meaningful to the community and appropriate for the scale of the project. We propose the following:

- **ITC Park Improvements:** A \$40,000 contribution toward improvements at the nearby ITC Park. The specific improvements to be provided will be based on the current needs of the park, as determined by the City Parks Department. Based on preliminary discussions with the Parks Department, known needs at ITC Park include the following-
  - New picnic tables, benches, and trash cans
  - New basketball hoops
  - Resurfacing of the existing basketball court
  - Paving of additional walkways
- **Concept planning for the ITC park expansion:** As an alternative to the physical improvements outlined above, the applicant will cover the cost of concept planning for the planned park expansion on the newly acquired 88-acre site directly east of the existing park. It is anticipated that the concept planning would cost approximately \$20,000. If the City would like to pursue the concept planning option, the cost of the concept planning would be subtracted from the total \$40,000 pledge. Any remaining balance would be spent on physical park improvements outlined above.

We appreciate the City's continued review and assistance with this project. We will remain committed to working closely with the City to ensure a smooth and timely review process. Should you have any remaining questions regarding the public benefits or other aspects of the project, please do not hesitate to contact me directly at (947) 886-9874.

Sincerely,  
**ATWELL, LLC**

A handwritten signature in blue ink, appearing to read "Chris Rothhaar".

Chris Rothhaar, P.E.  
Land Development- Project Manager

## PLANNING REVIEW

---



# PLAN REVIEW CENTER REPORT

## Planning Review

July 18, 2025

### JZ24-43 Mariella Estates PRO

(fka Preserves of Maybury)

Zoning Map Amendment No. 18.750

#### APPLICANT

Braciolo Brothers, Inc

#### REVIEW TYPE

Formal PRO Plan

Rezoning Request from RA Residential Acreage to R-1 One Family Residential with a Planned Rezoning Overlay

#### PROPERTY CHARACTERISTICS

Section	31	
Site Location	West of Garfield Road and North of Eight Mile Road (Parcel 22-31-400-008)	
Site School District	Northville Community School District	
Site Zoning	RA Residential Acreage	
Adjoining Zoning	North	RA Residential Acreage
	East	RA Residential Acreage
	West	RA Residential Acreage
	South	(Northville Township) Maybury State Park
Current Site Use	Farmland	
Adjoining Uses	North	Single Family Residential (under construction)
	East	Single Family Residential
	West	Single Family Residential (under construction)
	South	Maybury State Park/Single Family Residential
Site Size	9.36 acres	
Plan Date	June 27, 2025	

#### PROJECT SUMMARY

The subject property is located on the north side of Eight Mile Road, west of Garfield Road in Section 31 of the City of Novi. The property to be rezoned totals about 9.36 acres and is currently vacant. The applicant is proposing to develop a 10-unit single family residential development. The development proposes a private street with one entrance off Eight Mile Road with a gated entrance. The applicant is requesting to rezone the site from RA Residential Acreage to R-1 One Family Residential with a Planned Rezoning Overlay.

#### PRO OPTION

The PRO option creates a "floating district" with a conceptual plan attached to the rezoning of a parcel. As part of the PRO, the underlying zoning is proposed to be changed (in this case from RA

to R-1), and the applicant submits a conceptual plan for development of the site. After Staff and consultant review, the proposed request goes through initial review by the Planning Commission and City Council. Each of those bodies will provide feedback and comments on whether the project meets the eligibility criteria for the PRO process.

The applicant can then make any changes to the Concept Plan based on the feedback received, and resubmit for formal review. The Planning Commission holds a public hearing and makes a recommendation to City Council. The City Council reviews the Concept Plan, and if the plan receives tentative approval, it directs the preparation of an agreement between the City and the applicant, which also requires City Council approval. Following final approval of the PRO concept plan and PRO agreement, the applicant will submit for Preliminary and Final Site Plan approval under standard site plan review procedures. The PRO runs with the land, so future owners, successors, or assignees are bound by the terms of the agreement, absent modification by the City of Novi, or unless otherwise stated in the agreement or terminated.

## RECOMMENDATION

---

**Staff recommends approval of the Formal PRO Plan.** There will not be a significant change in the number of units as a result of the rezoning, with lots that are similar in size to the RUD developments adjacent. The three deviations requested are minor and will not detract from the development. The benefits of rezoning appear to outweigh the detriments.

## PROJECT HISTORY

---

The project was submitted and reviewed by staff and consultants in a pre-application submittal in January 2024. Comments were provided on the concept plans submitted, but no recommendations for approval were made at that time.

The initial PRO plan was submitted and reviewed in December 2024, with a revised initial PRO plan submitted and reviewed in March 2025. The Planning Commission held a public hearing on April 23, 2025 and provided feedback on the proposal. On May 19, 2025, City Council considered the request and provided feedback to the applicant. Minutes from both meetings are included as attachments to this letter.

## REVIEW CONCERNS

---

This project was reviewed for conformance with the Zoning Ordinance with respect to Article 3 (Zoning Districts), Article 4 (Use Standards), Article 5 (Site Standards), Section 7.13 (Amendments to Ordinance) and any other applicable provisions of the Zoning Ordinance. **Please see the attached chart for additional information pertaining to ordinance requirements.** Items in **bold** below must be addressed and incorporated as part of the next submittal:

1. Supporting Documentation: The applicant has provided the following as part of their application packet:
  - a. **Narrative:** The narrative provided states that Rezoning allows for development consistent with the adjacent communities that have developed under a Residential Unit Development (RUD) Agreement – Ballyntine and Parc Vista. This property is not eligible for an RUD as it is less than 20 acres in size, therefore the applicant is seeking a rezoning to R-1 in order to create similar-sized lots. **The applicant indicates 6 lots could be feasibly developed under the existing RA standards when providing an access road and stormwater detention. A “Parallel Plan” has been provided to demonstrate the potential lot layout under RA standards. The change in Residential Equivalent Units (REUs) would be from 6 under the current zoning to 10 with the proposed zoning.**
  - b. The statement also notes the conditions and deviations proposed, as well as public benefits. **Proposed benefit statement and deviation request received.**

- c. **Traffic Impact Study:** Not required as does not meet the threshold (rezoning to residential category two or more higher).
  - d. **Sign Location Plan:** A sign placement plan and rezoning sign mock-up have been provided for review. **Approved signage was placed as required.**
2. Wetland Delineation: The applicant has provided a statement from Atwell indicating the likely presence of a wetland on the site prior to grading that was conducted earlier, however that evaluation was based on a February site visit when no vegetation was growing, and no soil borings were taken. The wetland area has been delineated, and is now shown on the plans, and extends onto Lot 1. **The applicant has provided a wetland delineation and endangered species assessment prepared by Atwell, dated June 26, 2025. See the Wetland Review for detailed comments on these documents. Further, the applicant has proposed a boulder wall within the wetland buffer on Lot 1, as well as wetland buffer signage in two locations to discourage disturbance of the buffer. Both the boulder wall and the signage should be relocated to be at the edge of the 25-foot buffer to more effectively delineate the buffer area. Additional signs should also be placed, and the buffer shall be planted with a native seed mix as mentioned in the Wetland Review. Staff suggests a conservation easement over the wetland area as an additional benefit in the interest of the general public.**
3. Wetland Boundary on Lot 1: As mentioned above, the wetland boundary extends onto Lot 1 of the proposed plan. The Wetland and Watercourse Protection ordinance states that lot boundaries not extend into wetland areas (Section 12-174.(4) of City Code): "Where the proposed activity is the development of a site condominium, the boundaries of building sites, as defined in section [6.3] of the City of Novi Zoning Ordinance, shall not extend into a wetland or watercourse. This shall not prohibit the inclusion of wetland or watercourse areas within a site condominium development." **The City also prefers to keep 25-foot wetland buffers out of private lot boundaries to better protect them from disturbance. The applicant will need to adjust the Lot 1 boundaries to exclude the delineated wetland, which would not cause new deviations for lot width, but may affect the lot area. The applicant shall calculate the lot area of the redrawn Lot 1 to determine if any additional deviations need to be requested, or other lot adjustments need to be made.**
4. Active Mobility Plan: The City's Active Mobility Plan, adopted in 2024, recommends improvements to the Eight Mile Road crosswalk (flashing beacons or HAWK signal), and coordinating with Maybury State Park to provide a non-motorized connection between the crosswalk and the park entrance. Currently cyclists and pedestrians must use the narrow shoulder on the south side of 8 Mile to access the entrance to the park. While the south side of 8 Mile is in Northville Township, improved safety enhancements to access this important regional recreational destination would benefit Novi residents as well. **Based on feedback from City Council, the applicant is no longer proposing any modifications to the existing crosswalk on Eight Mile Road. See ITC Park improvements now proposed on page 9.**
5. Plan Review Chart: **The attached chart provides additional comments on many of the Ordinance review standards. Please refer to it in detail and note deviations that may be required if not corrected in the Formal PRO submittal.**
6. Other Reviews:
  - a. **Engineering:** Engineering recommends approval of the Formal PRO Plan. Negative impacts to public utilities are not expected with the requested change in zoning. Additional comments shall be addressed in the Site Plan submittal.
  - b. **Landscape:** Landscape review recommends approval with minor comments to be address during site plan review.

- c. **Traffic:** Traffic review notes that the applicant would need a deviations for centerline radius of the proposed private road, which are below the City standards. Approval is recommended.
- d. **Woodlands:** The site does not contain regulated woodlands. However, there are 4-5 trees on the site that are regulated as they are 36 inches diameter or greater. A woodland permit would be required for their removal.
- e. **Wetlands:** Wetlands review recommends approval. No impact to the wetland are indicated, however additional enhancements to the wetland buffer are suggested.
- f. **Façade:** No elevations of future homes have been provided for review.
- g. **Fire:** Fire has some additional concerns to be addressed in future submittals. Conditional approval is recommended.

**LAND USE AND ZONING: FOR SUBJECT PROPERTY AND ADJACENT PROPERTIES**

Figure 1: Current Zoning

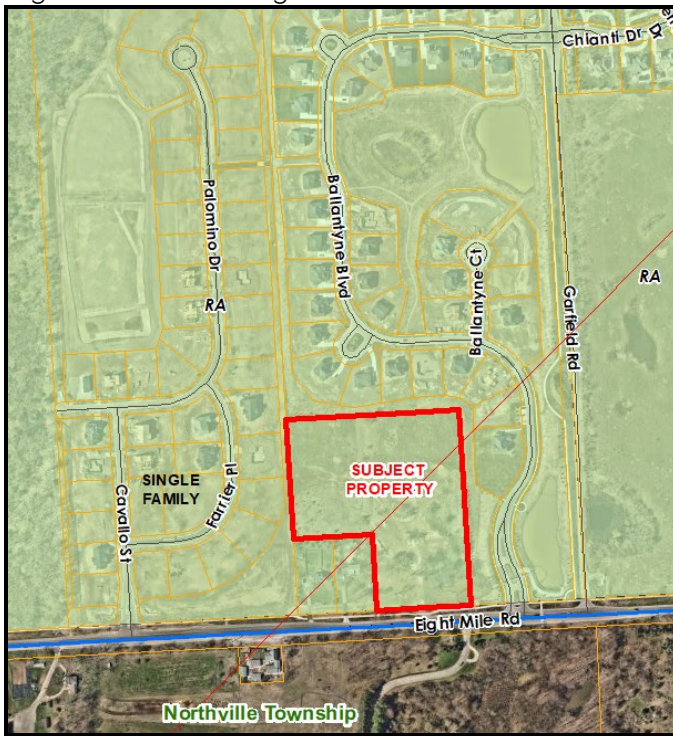
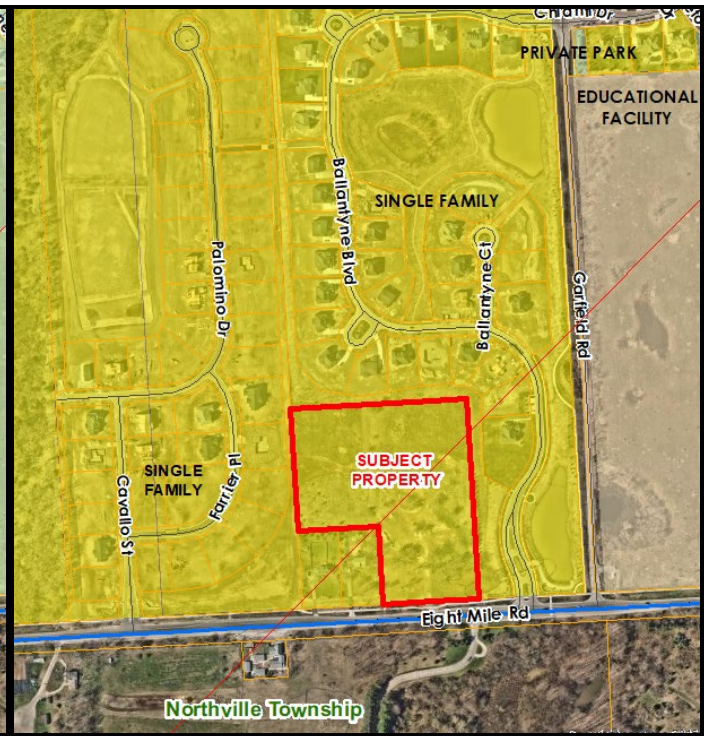


Figure 2: Future Land Use



The following table summarizes the zoning and land use status for the subject property and surrounding properties.

	Existing Zoning	Existing Land Use	Master Plan Land Use Designation
<b>Subject Property</b>	Residential Acreage	Vacant	Single Family
<b>Northern Parcels</b>	Residential Acreage	Single family homes	
<b>Eastern Parcels</b>	Residential Acreage	Single family homes	Single Family
<b>Western Parcels</b>	Residential Acreage	Single family homes	Single Family
<b>Southern Parcels</b>		State Parkland	Public Park

**Compatibility with Surrounding Land Use**

The subject property is located along the north side of Eight Mile Road, west of Garfield Road. It is surrounded by single family lots and subdivisions. The area to the south of Eight Mile is Maybury State Park in Northville Township. The proposed use consistent with the surrounding existing uses.

The applicant's narrative notes that they have attempted to create a layout that is similar to the adjacent new-construction communities that developed under the Residential Unit Development (RUD) provisions of the Ordinance. Because the subject property is less than 20 acres in size, it is not eligible for RUD consideration.



Figure 3: Names of surrounding developments

**Comparison of Zoning Districts**

The following table provides a comparison of the current (RA) and proposed (R-1) zoning classifications. The two districts are not significantly different from one another in terms of the types of uses allowed and building style permitted for homes. Differences are noted in bold text.

	<b>RA (EXISTING)</b>	<b>R-1 (PROPOSED)</b>
Principal Permitted Uses	One-family dwellings Farms and Greenhouses Publicly owned and operated parks, parkways and outdoor recreation facilities Home occupations Family day care homes Accessory buildings and uses customarily incidental to any of the	One-family dwellings Farms and Greenhouses Publicly owned and operated parks, parkways and outdoor recreation facilities Home occupations Family day care homes Accessory buildings and uses customarily incidental to any of the



	<b>RA (EXISTING)</b>	<b>R-1 (PROPOSED)</b>
	above uses	above uses
Special Land Uses	<ul style="list-style-type: none"> <li>- <b>Raising of nursery plant materials</b></li> <li>- <b>Dairies</b></li> <li>- <b>Keeping and raising of livestock</b></li> <li>- Places of worship</li> <li>- Public, parochial and private elementary, intermediate or secondary schools</li> <li>- Utility and public service buildings and uses (without storage yards)</li> <li>- Group day care homes, day care centers, and adult day care</li> <li>- Private noncommercial recreational areas, institutional or community recreation centers, nonprofit swimming pool clubs</li> <li>- Golf courses</li> <li>- Colleges, universities, and other such institutions of higher learning</li> <li>- Private pools</li> <li>- Cemeteries</li> <li>- Railroad right-of-way, but not including terminal freight facilities, transfer and storage tracks</li> <li>- Mortuary establishments</li> <li>- Bed and Breakfasts</li> <li>- <b>Limited non-residential use of historic buildings</b></li> <li>- Accessory buildings and uses incidental to the above</li> </ul>	<ul style="list-style-type: none"> <li>- Places of worship</li> <li>- Public, parochial and private elementary, intermediate or secondary schools</li> <li>- Utility and public service buildings and uses (without storage yards)</li> <li>- Group day care homes, day care centers, and adult day care</li> <li>- Private noncommercial recreational areas, institutional or community recreation centers, nonprofit swimming pool clubs</li> <li>- Golf courses</li> <li>- Colleges, universities, and other such institutions of higher learning</li> <li>- Private pools</li> <li>- Cemeteries</li> <li>- Railroad right-of-way, but not including terminal freight facilities, transfer and storage tracks</li> <li>- Mortuary establishments</li> <li>- Bed and Breakfasts</li> <li>- Accessory buildings and uses incidental to the above</li> </ul>
Lot Size	<b>1 acre</b>	<b>21,780 sf (1/2 acre)</b>
Lot Width	<b>150 feet</b>	<b>120 feet</b>
Lot Coverage	25%	25%
Building Height	35 ft or 2.5 stories, whichever is less	35 ft or 2.5 stories, whichever is less
Building Setbacks	<b>Front: 45 feet</b> <b>Rear: 50 feet</b> <b>Side: 20 feet min, total 50 ft two sides</b>	<b>Front: 30 feet</b> <b>Rear: 35 feet</b> <b>Side: 15 feet min, total 40 ft two sides</b>
Minimum Floor Area	1000 sf	1000 sf
Dwelling unit density maximum	<b>0.8 dwellings/acre</b>	<b>1.65 dwellings/acre</b>

**DEVELOPMENT POTENTIAL**

The land is currently vacant. Development under the current Residential Acreage zoning could result in 7 single family lots, based strictly on density permitted. However, the parallel plan provided shows only 6 lots could be fit onto the site given road, lot and stormwater configuration.

The current concept plan proposes the development of 10 single family lots (density of 1.07 dwellings per acre) for a single-family development, which is below the 1.65 dwellings/acre maximum density allowed in the R-1 zoning district. The lots all meet the ½-acre minimum lot size, and all comply with the minimum lot width except units 4 and 5 which are somewhat narrower than the 120-foot minimum due to being on the corner of the road. While the provision of "open space" is not required for standard R-1 developments, the proposed plan includes 2.65 acres of open space within the development to mimic what was provided with the adjacent RUD developments.

This includes a 25-foot buffer between the lot areas and the adjacent developments, which matches similar buffers on in those developments.

The RUD option allows the City Council to approve deviations from lot size requirements if the development dedicates a portion of the overall land for open space/preservation of natural features. However, the overall density cannot exceed the underlying zoning district. The adjacent Ballantyne and Parc Vista developments both have RUD Agreements that allowed a majority of the lots to meet R-1 District standards (rather than the R-A standards) in exchange for the preservation of a significant area of open space and/or existing wetland and woodland areas. The Parc Vista development preserved 44% of the site and Ballantyne preserved 35.7% of the site as open space while maintaining an overall density of 0.8 dwellings per acre.

The Master Plan for Land Use states the anticipated density for this area is 0.8 dwellings per acre, which is consistent with the existing Residential Acreage zoning.

The applicant provides a reasonable justification for the change of use, but greater density is proposed and less open space is maintained compared to the adjacent developments. This is generally due to the smaller area of land available for development. The small wetland area is now proposed to be retained. There are relatively few deviations required for this proposed development.<sup>1</sup>

## **2016 MASTER PLAN FOR LAND USE: GOALS AND OBJECTIVES**

The proposed use is currently not recommended by the 2016 Master Plan for Land Use. The following objectives as listed in the Master Plan are applicable for the proposed development. However, at this time the plan follows only a few. **The applicant should consider revisions to the plan to comply with as many goals as possible. Please refer to staff comments in bold and revisions recommended in bold and underline.**

### **1. General Goal: Quality and Variety of Housing**

- a. Provide residential developments that support healthy lifestyles. Ensure the provision of neighborhood open space within residential developments. **The development proposes the required sidewalks along the private street. A portion of the site is to remain undeveloped in open space.**
- b. Safe housing and neighborhoods. Enhance the City of Novi's identity as an attractive community in which to live by maintaining structurally safe and attractive housing choices and safe neighborhoods.
- c. Maintain existing housing stock and related infrastructure. **No homes would be removed to create the development.**
- d. Provide a wide range of housing options. Attract new residents to the City by providing a full range of quality housing opportunities that meet the housing needs of all demographic groups including but not limited to singles, couples, first time home buyers, families and the elderly. **The development would provide additional housing options in Novi.**

### **2. General Goal: Community Identity**

---

<sup>1</sup> The developer has committed to a contribution of \$40,000 to be used on improvements to the nearby ITC Park should the development proceed. The applicant proposes to work with the Parks Department to either make some specific improvements to be determined by the Parks Department or in connection with the ITC Park expansion. **The letter from Parks, Recreation and Cultural Services Director, Jeff Muck, indicates support for either of these options to benefit ITC Park.**

- a. Maintain quality architecture and design throughout the City. **No architectural drawings have been provided.**
- 3. General Goal: Environmental Stewardship**
- a. Protect and maintain the City's woodlands, wetlands, water features, and open space. The concept plan proposes additional removal of regulated woodlands. **Please refer to the wetland review letter for opportunities to further protect this natural feature.**
  - b. Increase recreational opportunities in the City. **The applicant proposes to contribute to ITC Park with the aim of furthering this goal.**
  - c. Encourage energy-efficient and environmentally sustainable development through raising awareness and standards that support best practices. **The applicant should consider sustainable, energy-efficient and best-practice design for site elements and building materials, such as LEED recommended strategies.**
- 4. General Goal: Infrastructure**
- a. Provide and maintain adequate water and sewer service for the City's needs. **Please refer to the Engineering memo.**
  - b. Provide and maintain adequate transportation facilities for the City's needs. Address vehicular and non-motorized transportation facilities. **A private street with sidewalks is proposed.**
- 5. General Goal: Economic Development / Community Identity**
- a. Ensure compatibility between residential and non-residential developments. **Please refer to comments about compatibility with surrounding development earlier in this review.**

## **ORDINANCE DEVIATIONS**

Section 7.13.2.D.i.c(2) permits deviations from the strict interpretation of the Zoning Ordinance within a PRO agreement. These deviations must be accompanied by a finding by City Council that ***"each Zoning Ordinance provision sought to be deviated would, if the deviation were not granted, prohibit an enhancement of the development that would be in the public interest, and that approving the deviation would be consistent with the Master Plan and compatible with the surrounding areas."*** Such deviations must be considered by City Council, who will make a finding of whether to include those deviations in a proposed PRO agreement. A proposed PRO agreement would be considered by City Council only after tentative approval of the proposed concept plan and rezoning.

The Concept Plan submitted with an application for a rezoning with a PRO is not required to contain the same level of detail as a preliminary site plan. Staff has reviewed the applicant's Concept Plan in as much detail as possible to determine what deviations from the Zoning Ordinance are currently shown. The applicant may choose to revise the concept plan to better comply with the standards of the Zoning Ordinance, or may proceed with the plan as submitted with the understanding that those deviations would have to be approved by City Council in a proposed PRO agreement. The previous concept plan required 8 deviations. The revised submittal has reduced that number to 6.

The following are Ordinance deviations that have been requested by the applicant:

1. **Lot Width (Sec 3.1.2.D):** A Zoning Ordinance deviation is requested to reduce the required lot width for lots 4 and 5 to 98 feet (120 feet required). *The deviation is requested for the two pie-shaped lots near the corner of the road.*  
**Applicant Justification:** *These lots still provide adequate space for the intended housing product, are the two largest lots proposed, and exceed the R-1 minimum lot area requirements for over 10,000 square feet and 5,000 square feet respectively.*

2. **Lack of Cul-de-Sac** (Code of Ord, Figure VIII-F): A deviation is required to provide a T-turnaround in lieu of a cul-de-sac at the end of the roadway. **(Note: this deviation is no longer required as the road design meets the appropriate standards.)**  
*Applicant Justification:* Given the low volume of traffic that this subdivision will encounter a T-turnaround is being proposed due to geometric constraints and a way to reduce total pavement on site. The dimensions of the proposed turnaround meets current International Fire Code requirements.
3. **Road Centerline Radius** (Code of Ordinances, Sec. 11-194.b.2): Design and Construction Standards deviation for proposed street with 90-foot centerline radius (230-foot radius standard).  
*Applicant Justification:* This does not provide a safety concern given the short distance of the road, the low travel speed, and the minimal traffic volumes expected.

All deviations from the ordinance requirements shall be identified and included in PRO Agreement. Any additional deviations identified during Site Plan Review (after the Concept Plan and PRO Agreement is approved), will require amendment of the PRO Agreement.

#### **APPLICANT'S BURDEN UNDER PRO ORDINANCE**

The Planned Rezoning Overlay ordinance (PRO) requires the applicant to demonstrate that certain requirements and standards are met. The applicant should be prepared to discuss these items, especially in number 1 below, where the ordinance suggests that the enhancement under the PRO request would be unlikely to be achieved or would not be assured without utilizing the Planned Rezoning Overlay. Section 7.13.2.D.ii states the following:

1. (Sec. 7.13.2.D.ii.a) *The PRO accomplishes the integration of the proposed land development project with the characteristics of the project area in such a manner that results in an enhancement of the project area as compared to the existing zoning that would be unlikely to be achieved or would not be assured in the absence of the use of a Planned Rezoning Overlay.*
2. (Sec. 7.13.2.D.ii.b) *Sufficient conditions shall be included on and in the PRO Plan and PRO Agreement such that the City Council concludes, in its discretion, that, as compared to the existing zoning and considering the site specific land use proposed by the applicant, it would be in the public interest to grant the Rezoning with Planned Rezoning Overlay. In determining whether approval of a proposed application would be in the public interest, the benefits which would reasonably be expected to accrue from the proposal shall be balanced against, and be found to clearly outweigh the reasonably foreseeable detriments thereof, taking into consideration reasonably accepted planning, engineering, environmental and other principles, as presented to the City Council, following recommendation by the Planning Commission, and also taking into consideration the special knowledge and understanding of the City by the City Council and Planning Commission.*

#### **The applicant has listed the following conditions in the public interest for consideration:**

1. **Perimeter Landscape Buffers:** The development proposes a minimum 25-foot perimeter landscape buffer from the lots to the property boundary. **There are approximately 70 trees proposed to be planted to enhance this buffer.**
2. **Increased Open Space:** The development proposes an extensive amount of open space for a single-family development (28%) and a majority of the proposed open space is usable

active open space. **Compared to the adjacent RUD developments, this proposal has a lower percentage of open space preserved, but providing open space in the R-1 District is not required, so this is an enhancement beyond what would typically be required. The wetland area near 8 Mile is now proposed for preservation.**

3. **Reduced Density:** Overall density shall not exceed 1.07 dwelling units per acre. **This would be more limiting than the 1.6 dwelling units per acre allowed in the R-1 District, and closer to what has been developed in the surrounding neighborhoods.**
4. **Wetland Preservation:** Impacts to the small wetland on site are not proposed. **The applicant should consider placing the wetland in a conservation easement, which would be considered an additional enhancement to the project.**
5. **Wetland Buffer Protection:** The applicant has included two signs to indicate the presence of the wetland buffer and discourage disturbance. There is also a boulder wall indicated on Lot 1 within the wetland buffer. **Both the signage and the wall should be relocated to the outer edge of the 25-foot buffer to more effectively limit disturbance, including mowing, cutting, planting and removal of vegetation. The number of signs should also be increased to allow placement at 50-foot intervals.**

**This is a PRO in which the applicant seeks both a rezoning and a list of ordinance deviations. The Planning Commission and City Council should offer their thoughts on whether the proposed benefits are sufficient.**

#### **NEXT STEP: PLANNING COMMISSION PUBLIC HEARING**

---

With all reviewers recommending approval or conditional approval, Planning Commission will hold a public hearing on the rezoning request from RA (Residential Acreage) to R-1 (One Family Residential) with a Planned Rezoning Overlay. Following the public hearing, they will make a recommendation to City Council whether to approve or deny the request, or may postpone making a recommendation if they determine additional information or changes are needed.

The next available date for the Planning Commission for the public hearing on the Formal PRO Plan is Wednesday, August 20, 2025. **Please provide a response letter that addresses all comments as needed, including a full list of deviations and conditions to be included in the PRO Agreement, no later than Wednesday, August 13<sup>th</sup>.**

#### **CITY COUNCIL CONSIDERATION**

---

After the Planning Commission makes its recommendation, the PRO Concept Plan will be scheduled for consideration by the City Council. If the City Council grants tentative approval at that time, they will direct the City Attorney to draft a PRO Agreement describing the terms of the rezoning approval. Once the PRO Agreement has been drafted and approved by the applicant's attorney, it will return City Council for final approval.

If the applicant has any questions concerning the above review or the process in general, do not hesitate to contact me at 248.347.0484 or [lbell@cityofnovi.org](mailto:lbell@cityofnovi.org).



---

Lindsay Bell, AICP, Senior Planner



## PLANNING REVIEW CHART: Rezoning to R-1 with PRO

**Review Date:** July 17, 2025  
**Review Type:** Formal PRO Plan  
**Project Name:** **JZ24-43 MARIELLA ESTATES PRO**  
**Plan Date:** June 27, 2025  
**Prepared by:** Lindsay Bell, AICP, Senior Planner  
**Contact:** **E-mail:** lbell@cityofnovi.org; **Phone:** (248) 347-0484

**Bold** To be addressed in Formal PRO Plan submittal  
Underline To be addressed with Preliminary Site Plan submittal  
**Bold and Underline** Possible deviations to be included as part of PRO agreement  
*Italics* Items to be noted

Item	Required Code	Proposed	Meets Code	Comments
<b>Zoning and Use Requirements</b>				
<b>Master Plan</b> <i>(adopted July 26, 2017)</i>	Single Family, with master planned 0.8 maximum dwelling units per acre.	10 Unit single family residential development (1.07 du/ac)	<b>No</b>	
<b>Zoning</b> <i>(Effective January 8, 2015)</i>	RA: Residential Acreage district	R-1 with PRO	<b>No</b>	<b>PRO Rezoning Requested</b>
<b>Uses Permitted</b> <i>(Sec.3.1.1)</i>	Single Family Dwellings	Single Family dwellings	Yes	
<b>Planned Rezoning Overlay Document Requirements</b> (SDM link: <a href="#">Site Plan &amp; Development Manual</a> )				
<b>Written Statement</b> <i>(Site Plan &amp; Development manual) The statement should describe the following</i>	Potential development under the proposed zoning and current zoning	Provided, including parallel plan	Yes	
	Identified benefit(s) of the development	Stated	Yes	
	Conditions proposed for inclusion in the PRO Agreement (i.e., Zoning Ordinance deviations, limitation on total units, etc.)	Stated	Yes	
<b>Sign Location Plan</b> <i>(Page 23, SDM)</i>	Installed within 15 days prior to public hearing Located along all road frontages	Provided and installed	Yes	
<b>Traffic Impact Study</b> <i>(Site Plan &amp; Development manual)</i>	A Traffic Impact Study as required by the City of Novi Site Plan and Development Manual.		NA	
<b>Community Impact Statement</b>	- Over 30 acres for permitted non-residential projects		NA	

Item	Required Code	Proposed	Meets Code	Comments
(Sec. 2.2)	- Over 10 acres in size for a special land use - All residential projects with more than 150 units A mixed-use development, staff shall determine			
<b>Market Study</b>	Optional: a Market study to provide a market demand analysis for the proposed project.		NA	
<b>R-1 One-Family Residential, Height, bulk, density and area limitations (Sec. 3.1.2)</b>				
<b>Lot Size</b> (Sec 3.1.2.D)	R-1 zoning requires: <ul style="list-style-type: none"> <li>21,780 sf lot area</li> <li>120 ft. lot widths</li> </ul>	Minimum area : 21,780 sf Lot width: 98-feet shown in lot table sheet 04 for lots 4 and 5	Yes  <b>No</b>	<b><u>Lots 4 and 5 require a deviation for lot width</u></b>
<b>Building Setbacks</b> (Sec 3.1.2.D)	<b>Front:</b> 30 ft.	30 ft	Yes	Proposed to comply based on building footprints
	<b>Side:</b> 15 ft. one side, 40 ft. two sides	40 total, 15-ft minimum	Yes	
	<b>Rear:</b> 35 ft.	35 ft	Yes	
<b>Maximum % of Lot Area Covered</b> (By All Buildings) (Sec 3.1.2)	25%	Not shown	TBD	<i>Details reviewed at plot plan phase</i>
<b>Minimum Floor Area</b> (Sec 3.1.2)	1,000 Sq.ft.	3,200-4,300 sq ft. per unit indicated	Yes	<i>Details reviewed at plot plan phase</i>
<b>Building Height</b> (Sec 3.1.2)	35 ft. or 2.5 stories whichever is less	35 feet, 2 stories indicated	Yes	<i>Details reviewed at plot plan phase</i>
<b>Frontage on a Public Street.</b> (Sec. 5.12)	No lot or parcel of land shall be used for any purpose permitted by this Ordinance unless said lot or parcel shall front directly upon a public street, unless otherwise provided for in this Ordinance.	All units front on a proposed private road within the proposed condominium, with access to Eight Mile Road	Yes	Frontage on Private road for individual lots is permitted for a Condominium development
<b>Note to District Standards (Sec 3.6)</b>				
<b>Area Requirements</b> (Sec 3.6A & Sec. 2.2)	- Lot width shall be measured between two lines where a front setback line intersects with side setback lines.	Lot widths clarified	Yes?	

Item	Required Code	Proposed	Meets Code	Comments
	- Distance between side lot lines cannot be less than 90% between the front setback line and the main building.			
<b>Additional Setbacks</b> (Sec 3.6.B)	NA	No off-street parking lots	NA	
<b>Exterior Side yard abutting Streets</b> (Sec 3.6.C)	NA	Side yards abutting residential districts	NA	
<b>Wetland/Water-course Setback</b> (Sec 3.6.M)	25ft. from boundary of a wetland and 25ft. from the ordinary highwater mark of a watercourse is required	Small wetland in SE corner of property – wetland buffer extends onto Lot 1	<b>Yes?</b>	<b>Lot 1 shall include buffer delineation to prevent encroachment/mowing/removal of vegetation</b>
<b>Subdivision Ordinance</b>				
<b>Blocks</b> (Subdivision Ordinance: Sec. 4.01)	- Maximum length for all blocks shall not exceed 1,400 ft. - Widths of blocks shall be determined by the conditions of the layout.	Small site, so blocks not longer than 1400 ft.	Yes	
<b>Lots: Sizes and Shapes</b> (Subdivision Ordinance: Sec. 4.02A)				
<b>Lot Depth Abutting a Secondary Thoroughfare</b> (Subdivision Ordinance: Sec. 4.02.A5)	Lots abutting a major or secondary thoroughfare must have a depth of at least 140'	No lots abutting 8 Mile	Yes	
<b>Depth to Width Ratio</b> (Subdivision Ordinance: Sec. 4.02.A6)	Single Family lots shall not exceed a 3:1 depth to width ratio	Maximum of 1.7:1 ratio is maintained	Yes	
<b>Arrangement</b> (Subdivision Ordinance: Sec. 4.02.B)	- Every lot shall front or abut on a street. - Side lot lines shall be at right angles or radial to the street lines, or as nearly as possible thereto.	- All lots front on proposed street - All lots conform to shape requirement	Yes	
<b>Streets</b> (Subdivision Ordinance: Sec. 4.04)	Extend streets to boundary to provide access intervals not to exceed 1,300 ft. unless one of the following exists:	No stub streets proposed – recent abutting developments do not have any connecting points to take advantage of	Yes	<b>Extension to the north and west is impractical as the approved subdivisions have no streets available for connection</b>



Item	Required Code	Proposed	Meets Code	Comments
	<ul style="list-style-type: none"> <li>- practical difficulties because of topographic conditions or natural features</li> <li>- Would create undesirable traffic patterns</li> </ul>			
<b>Topographic Conditions (Subdivision Ordinance Sec 4.03)</b>				
<b>A. Flood plain</b>	Compliance with applicable state laws and City Code Areas in a floodplain cannot be platted	Not Applicable	NA	
<b>B. Trees and Landscaping</b>	Compliance with Chapter 37 and Article 5 of City Zoning Code	Tree survey and Landscape Plans are provided	Yes	
<b>C. Natural Features</b>	To be preserved Lots cannot extend into a wetland or watercourse	Wetland appears to exist on southeast corner of the site – not delineated	<b>No?</b>	
<b>D. Man-made Features</b>	To be built according to City standards	Underground detention proposed	Yes	<b>See Engineering Review letter for detail on SWM Plan comments</b>
<b>E. Open Space Areas</b>	Any Open Space Area shall meet the following: <ul style="list-style-type: none"> <li>- Require performance guarantee</li> <li>- Shall be brought to a suitable grade</li> <li>- Compliance with zoning ordinance</li> <li>- Except for wooded areas, all ground area should be top dressed with a minimum of 25% of red fescue and a maximum of 20% perennial rye.</li> </ul>	Over 2.5 acres of open space are proposed	Yes	
<b>F. Non-Access Greenbelt Easements</b>	Along rear or side property lines for reverse frontage lots	75 ft greenbelt between 8 Mile and side yards of nearest lot	Yes	
<b>G. Zoning Boundary Screening</b>	A non-residential development abutting a residential development would need screening		NA	
<b>Sidewalks Requirements</b>				

Item	Required Code	Proposed	Meets Code	Comments
<a href="#">Active Mobility Plan</a>	AMP recommends upgrading nearby crosswalk and providing a trail connection to entrance of Maybury State Park; New subdivision entrances	Not proposed		
<b>Public Sidewalks</b> (Chapter 11, Sec.11-276(b), Subdivision Ordinance: Sec. 4.05)	An 8' wide public sidewalk shall be constructed along all arterial and collector roads except in industrial districts	8' sidewalk existing along Eight Mile Road within ROW	Yes	
<b>Other Requirements</b>				
<b>Development and Street Names</b>	Development and street names must be approved by the <a href="#">Street Naming Committee</a>	Mariella Estates proposed Mariella Lane proposed	<b>Yes</b>	<b>The committee has considered and approved the requested names</b>
<b>Development/ Business Sign</b>	Signage if proposed requires a permit.		<b>TBD</b>	<u>Sign permits are reviewed separately.</u>
<b>NOTES:</b>				
<ol style="list-style-type: none"> <li>1. This table is a working summary chart and not intended to substitute for any Ordinance or City of Novi requirements or standards.</li> <li>2. The section of the applicable ordinance or standard is indicated in parenthesis. Please refer to those sections in Article 3, 4 and 5 of the zoning ordinance for further details.</li> <li>3. Please include a written response to any points requiring clarification or for any corresponding site plan modifications to the City of Novi Planning Department with future submittals.</li> </ol>				



July 15, 2025

Lindsay Bell  
Senior Planner  
City of Novi Community Development

**CITY COUNCIL**

**Mayor**

Justin Fischer

**Mayor Pro Tem**

Laura Marie Casey

David Staudt

Brian Smith

Ericka Thomas

Matt Heintz

Priya Gurumurthy

**City Manager**

Victor Cardenas

**Director of Parks, Recreation &  
Cultural Services**

Jeffrey A. Muck

**Deputy Director**

Tracie Ringle

Lindsay,

I've reviewed the public benefits proposed for the Marielle Estates PRO by Atwell, LLC. Novi Parks, Recreation and Cultural Services is amenable to either option.

The acquisition of 88 acres to the east of ITC Community Sports Park does necessitate the need for a new conceptual plan for the entire park. We would provide Atwell with the conceptual plan that was completed for the original park property and work with them on identifying new features and amenities for future development.

If the Planning Commission and/or City Council would prefer the proposed improvements to the existing park, PRCS would work with our Parks and Municipal Property team to identify the areas of highest need and implement the improvements as soon as possible to benefit park users.

Thankyou and please let me know if I can be of further assistance.

Sincerely,

Jeffrey A. Muck, CPRP  
Director  
Novi Parks, Recreation & Cultural Services



**City of Novi**  
45175 Ten Mile Road  
Novi, Michigan 48375  
248.347.0400  
248.347.3286 fax

cityofnovi.org

## ENGINEERING REVIEW

---



# PLAN REVIEW CENTER REPORT

03/20/2025

## Engineering Review

Mariella Estates  
JSP24-0043

### APPLICANT

---

Braciolo Brothers LLC

### REVIEW TYPE

---

Revised Initial PRO Plan

### PROPERTY CHARACTERISTICS

---

- Site Location: Located on the north side of 8 Mile Road west of Garfield Road located in section 31 of the City of Novi
- Site Size: 9.36 acres
- Plan Date: 02/28/2025
- Design Engineer: Atwell Group

### PROJECT SUMMARY

---

- Planned Rezoning Overlay Site Plan (PRO): Currently zoned R-A (Residential Acreage), applicant wishes to rezone to R-1.
- Construction of a 10-unit single family residential home development. Site access would be provided via 8 Mile Road.
- Water service would be provided by an extension from the existing 12-inch water main along the north side of 8 Mile Road, along with 2 additional hydrants.
- Sanitary sewer service would be provided by an extension from the existing 8-inch sanitary sewer along the north side of 8 Mile Road.
- Storm water would be collected by a single storm sewer collection system and discharged to an on-site underground detention system.

### RECOMMENDATION

---

Approval of the revised Initial PRO Plan is **recommended** at this time, the plan meets the general requirements of the design and construction standards as set forth in [Chapter 11 of the City of Novi Code of Ordinances](#), the Storm Water Management Ordinance and the [Engineering Design Manual](#) with the following items to be addressed at the time of site plan submittal:

### COMMENTS

1. Based on the existing zoning RA zoning six homes could be proposed on this site, the new zoning will allow for 10 homes. Engineering does not have any concerns with the 4 additional REUs that would be allowed with this rezoning.
2. Applicant must obtain RCOC approval for the approach tapers prior to final PRO plan approval. Permit will not be required at that time, but applicant must have RCOC review the approach location. Engineering does not have concerns with the proposed approach tappers.
3. Sheet 04 Layout Notes state that the road will not be gated and that roads will be private, the plan shows a gate at the entrance and the road is labeled with 60' ROW. Clarify if roads will be private, revise note to state 60' private road ROW.
4. Provide a utility crossing table at the time of the site plan submittal.
5. Generally, all proposed trees shall remain outside utility easements. Where proposed trees are required within a utility easement, the trees shall maintain a minimum 5-foot horizontal separation from water main and storm sewer and 10-foot horizontal separation from sanitary sewer.
6. Indicate if an entrance streetlight shall be proposed. The City of Novi has a streetlighting program where the city pays for one standard streetlight at the entrance of subdivisions. This would be an agreement with DTE, if a decorative streetlight is proposed the applicant/HOA will need to pay the difference in cost.

### WATER MAIN

7. All public water main shall be within a dedicated water main easement.
8. Additional details shall be provided at time of site plan submittal. Profiles shall be needed at time of final site plan submittal.
9. EGLE water main permit will be required for the main extension, the application can be submitted at time of final site plan submittal.

### SANITARY SEWER

10. All public sanitary sewer shall be within a dedicated sanitary sewer easement.
11. Sanitary sewer should be relocated outside of the pavement to the east and north side of Mariella Lane.
12. EGLE sanitary sewer extension permit shall be required prior to the pre-con meeting, EGLE application can be submitted at time of final site plan submittal.

### STORM SEWER

13. A minimum cover depth of 3 feet shall be maintained over all proposed storm sewer. Provide profiles for all storm sewer 12-inch and larger at time of site plan submittal.
14. Provide Storm sewer basis of design table at time of site plan submittal.

### **STORM WATER MANAGEMENT PLAN**

15. The proposed outlet location is acceptable, the applicant should ensure that the existing driveway culverts have adequate capacity and are cleaned out as part of this project.
16. The Ballantyne storm water detention basin does not outlet to the wetlands near the entrance of Mariella Estates.
17. Provide supporting calculations for the runoff coefficient determination. C factor greenspace shall be based on hydrologic soil type.
18. Provide the overland routing that would occur in the event the underground system cannot accept flow. This route shall be directed to a recognized drainage course or drainage system.
19. Provide a soil boring in the vicinity of the proposed underground detention system to determine bearing capacity and the high-water elevation of the groundwater table.
20. Provide inspection ports throughout the underground detention system at the midpoint of all storage rows. Additional inspection ports may be required for systems larger than 200 feet. Inspection ports shall be a minimum of 8-inches.
21. For piped/chamber systems the underground storage system shall include 4-foot diameter manholes at one end of each row for maintenance access purposes. Manholes are shown on plans.
22. Provide critical elevations for the detention system. Also, provide a cross-section for the underground detention system. Ensure that there is at least 1 foot of freeboard between the 100-year elevation and the subgrade elevation beneath the proposed park area.
23. The underground detention system shall be kept outside of the influence of any planting areas. Show manhole locations on landscaping sheets.

### **PAVING & GRADING**

24. Provide a construction materials table on the Paving Plan listing the quantity and material type for each pavement cross-section being proposed.
25. Label specific ramp locations on the plans where the detectable warning surface is to be installed.
26. Provide existing and proposed contours on the Grading Plan at the time of the Final Site Plan submittal.
27. Site grading shall be limited to 1V:4H (25-percent), regrade or relocate the proposed home and driveway on lot 8 so that the slopes steeper than 1V:4H are not on the proposed on the residential lot.
28. Retaining walls that are 48-inches or larger shall need a permit from Building Department.
29. A retaining wall that has a grade change of 30" or more within a 3' horizontal distance will require a guardrail.
30. Soil borings along the proposed road will be required at 500-foot intervals per Section 11-195(d) of the Design and Construction Standards.

**OFF-SITE EASEMENTS**

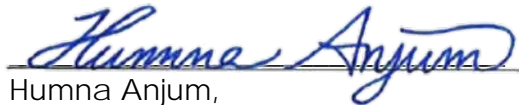
31. No off-site easements anticipated at this time.

**THE FOLLOWING MUST BE SUBMITTED WITH THE NEXT SUBMITTAL:**

32. A letter from either the applicant or the applicant's engineer must be submitted with the Stamping Set highlighting the changes made to the plans addressing each of the comments listed above and indicating the revised sheets involved. **Additionally, a statement must be provided stating that all changes to the plan have been discussed in the applicant's response letter.**

To the extent this review letter addresses items and requirements that require the approval of or a permit from an agency or entity other than the City, this review shall not be considered an indication or statement that such approvals or permits will be issued.

Please contact Humna Anjum at (248) 735-5632 or email at [hanjum@cityofnovi.org](mailto:hanjum@cityofnovi.org) with any questions.



Humna Anjum,  
Project Engineer

cc: Lindsay Bell, Community Development  
Milad Alesmail, Engineering  
Ben Croy, City Engineer



## LANDSCAPE REVIEW

---



# PLAN REVIEW CENTER REPORT

July 3, 2025  
Mariella Estates  
Formal PRO Concept Plan - Landscaping

## Review Type

Formal PRO Concept Plan Landscape Review

## Job #

JZ24-43

## Property Characteristics

- Site Location: 8 Mile Road west of Ballantyne
- Site Acreage: 9.36 ac.
- Site Zoning: RA
- Adjacent Zoning: North, East, West: RA, South: Maybury State Park
- Plan Date: 6/27/2025

## Ordinance Considerations

This project was reviewed for conformance with Chapter 37: Woodland Protection, Zoning Article 5.5 Landscape Standards, the Landscape Design Manual and any other applicable provisions of the Zoning Ordinance. Items in **bold** below must be addressed and incorporated as part of the Preliminary Site Plan submittal. Underlined items must be addressed on the Final Site Plans. Please follow guidelines of the Zoning Ordinance and Landscape Design Guidelines. This review and the accompanying Landscape Chart are summaries and are not intended to substitute for any Ordinance.

## **RECOMMENDATION:**

This project is **recommended for approval of the Formal PRO Plan**. Some minor additions and revisions are needed on the Final Site Plans.

**No landscape deviations are required for the proposed layout.**

## Ordinance Considerations

### Existing Trees (Sec 37 Woodland Protection, Preliminary Site Plan checklist #17 and LDM 2.3 (2))

1. Tree survey is provided.
2. There are no regulated woodlands on the site but some regulated trees that are 36" dbh or greater are on the site and are shown as being removed.
3. Woodland replacement calculations are provided, and all 10 of the required replacements are shown as being planted on the site.
4. There is a pond at the southeast corner of the site that extends into Lot 1. **See the Merjent and Planning letter regarding this pond.**

### Adjacent to Residential - Buffer (Zoning Sec. 5.5.3.B.ii and iii)

1. The project is only adjacent to other single-family residential property so no screening between the developments is required.
2. A dense evergreen hedge is proposed on the west end of the drive and screening trees are proposed between the project and Ballantyne, and the residents south of lots 8 and 9.

Adjacent to Public Rights-of-Way – Berm/Wall, Buffer and Street Trees (Zoning Sec. 5.5.3.B.ii, iii)

1. Both required berms are proposed, except in the pond frontage. Please extend the western berm to the west as much as possible to enhance the screening.
2. All greenbelt landscaping requirements are met or exceeded by the proposed plantings.
3. The required street trees are provided.

Interior Street Trees (Zoning Sec 5.5.3.F.iii)

The required street trees are shown.

Plant List (LDM 4, 10)

Please provide a plant list on the Final Site Plans at the very latest.

Planting Notations and Details (LDM 10)

Provided

Storm Basin Landscape (Zoning Sec 5.5.3.E.iv and LDM 3)

Underground detention is proposed. If that is approved by engineering, no detention basin landscaping is required.

Irrigation (LDM 10)

1. If an irrigation system will be used, a plan for it must be provided with Final Site Plans.
2. If alternative means of providing water to the plants for their establishment and long-term survival, information regarding that is also required with Final Site Plans.

If the applicant has any questions concerning the above review or the process in general, do not hesitate to contact me at 248.735.5621 or [rmeader@cityofnovi.org](mailto:rmeader@cityofnovi.org).



---

Rick Meader – Landscape Architect

## LANDSCAPE REVIEW SUMMARY CHART – Formal PRO Concept Plan

Project name: JZ24-43: Mariella Estates  
 Property location: 8 Mile Road, west of Ballantyne subdivision  
 Plan Date: June 27, 2025  
 Review Date: July 3, 2025  
 Reviewed by: Rick Meader, Landscape Architect, LLA [rmeader@cityofnovi.org](mailto:rmeader@cityofnovi.org), (248) 735-5621

Items in **Bold** need to be addressed by the applicant before approval of the Preliminary Site Plan.  
Underlined items need to be addressed on the Final Site Plan.

**There are no landscape deviations required for the proposed plan.**

Item	Required	Proposed	Meets Code	Comments
<b>Landscape Plan Requirements (LDM (2))</b>				
<b>Landscape Plan</b> (Zoning Sec 5.5.2, LDM 2.e.)	<ul style="list-style-type: none"> <li>• New commercial or residential developments</li> <li>• Addition to existing building greater than 25% increase in overall footage or 400 SF whichever is less.</li> <li>• 1"=20' minimum with proper North. Variations from this scale can be approved by LA</li> <li>• Consistent with plans throughout set</li> </ul>	<ul style="list-style-type: none"> <li>• Overall: 1" = 50'</li> <li>• Greenbelt plan: 1" = 30'</li> </ul>	Yes	
<b>Project Information</b> (LDM 2.d.)	Name and Address	Location map provided	Yes	
<b>Owner/Developer Contact Information</b> (LDM 2.a.)	Name, address and telephone number of the owner and developer or association	On title block	Yes	
<b>Landscape Architect contact information</b> (LDM 2.b.)	Name, Address and telephone number of RLA/PLA/LLA who created the plan	Jim Allen – Allen Design	Yes	
<b>Sealed by LA.</b> (LDM 2.g.)	Requires original signature	Copy of signature and seal	Yes	
<b>Miss Dig Note</b> (800) 482-7171 (LDM.3.a.(8))	Show on all plan sheets	On title block	Yes	
<b>Zoning</b> (LDM 2.f.)	Include all adjacent zoning	<u>Shown on location map</u> Site: RA North, East & West: RA South: Northville Twp	Yes	

Item	Required	Proposed	Meets Code	Comments
<b>Survey information</b> (LDM 2.c.)	<ul style="list-style-type: none"> <li>• Legal description or boundary line survey</li> <li>• Existing topography</li> </ul>	<ul style="list-style-type: none"> <li>• Sheet 2</li> </ul>	<ul style="list-style-type: none"> <li>• Yes</li> </ul>	
<b>Existing plant material</b> <b>Existing woodlands or wetlands</b> (LDM 2.e.(2))	<ul style="list-style-type: none"> <li>• Show location type and size. Label to be saved or removed.</li> <li>• Plan shall state if none exists.</li> </ul>	<ul style="list-style-type: none"> <li>• Tree survey on L-4</li> <li>• All tree removals are indicated on L-4</li> <li>• Woodland replacement calculations and 10 trees are provided</li> </ul>	<ul style="list-style-type: none"> <li>• Yes</li> <li>• Yes</li> <li>• Yes</li> </ul>	<ol style="list-style-type: none"> <li>1. Please provide a current wetland delineation.</li> <li>2. See the Merjent review for a complete discussion of the trees and wetlands.</li> <li>3. Will trees #2401 and #2402 be removed? It appears they are within the wetland that is not being changed.</li> </ol>
<b>Soil types</b> (LDM.2.r.)	<ul style="list-style-type: none"> <li>• As determined by Soils survey of Oakland Co.</li> <li>• Show types, boundaries</li> </ul>	Sheet 2 – Fox Sandy loam and Glynwood loam	Yes	
<b>Existing and proposed improvements</b> (LDM 2.e.(4))	Existing and proposed buildings, easements, parking spaces, vehicular use areas, and R.O.W	Yes	Yes	
<b>Existing and proposed utilities</b> (LDM 2.e.(4))	<ul style="list-style-type: none"> <li>• Overhead and underground utilities, including hydrants</li> <li>• Show all proposed light posts</li> </ul>	<ul style="list-style-type: none"> <li>• Proposed storm water system, including underground storage, is shown</li> <li>• Water and sanitary lines and structures are also shown</li> <li>• Sufficient spacing for the street trees appears to be proposed.</li> </ul>	<ul style="list-style-type: none"> <li>• Yes</li> <li>• Yes</li> <li>• Yes</li> </ul>	
<b>Proposed grading. 2' contour minimum</b> (LDM 2.e.(1))	Provide proposed contours at 2' interval	Sheet 6	Yes	
<b>Snow deposit</b> (LDM.2.q.)	Show snow deposit areas on plan	<ul style="list-style-type: none"> <li>• No areas are shown</li> <li>• A note indicates that snow will be deposited along the road, between the curb and sidewalk</li> </ul>	TBD	

**LANDSCAPING REQUIREMENTS**  
**Parking Area Landscape Requirements LDM 1.c. & Calculations (LDM 2.o.)**

Item	Required	Proposed	Meets Code	Comments
<b>General requirements</b> (LDM 1.c)	<ul style="list-style-type: none"> <li>• Clear sight distance within parking islands</li> <li>• No evergreen trees</li> </ul>	No parking areas are required or proposed		
<b>Name, type and number of ground cover</b> (LDM 1.c.(5))	As proposed on planting islands	NA		
<b>General (Zoning Sec 5.5.3.C.ii)</b>				
<b>Parking lot Islands</b> (a, b. i)	<ul style="list-style-type: none"> <li>• A minimum of 300 SF to qualify</li> <li>• 6" curbs</li> <li>• Islands minimum width 10' BOC to BOC</li> </ul>	NA		
<b>Curbs and Parking stall reduction</b> (c)	Parking stall can be reduced to 17' and the curb to 4" adjacent to a sidewalk of minimum 7ft.	NA		
<b>Plantings around Fire Hydrant</b> (d)	No plantings with matured height greater than 12' within 10 ft. of fire hydrants or other utility structures, and 5 feet from underground utility lines	It appears that trees are correctly spaced from structures and utility lines	Yes	
<b>Landscaped area</b> (g)	Areas not dedicated to parking use or driveways exceeding 100 sq. ft. shall be landscaped	NA		
<b>Clear Zones</b> (LDM 2.3.(5))	<ul style="list-style-type: none"> <li>• 25 ft corner clearance required at Garfield Road entry. Refer to diagram from Section 5.5.9</li> <li>• RCOC guidelines to determine required clear vision zone for 8 Mile Road entry.</li> </ul>	The RCOC clear zone is shown and no trees are located within it	Yes	<u>If the RCOC does not allow any or all of the street trees shown along 8 Mile Road, they do not need to be planted, but a copy of their decision must be provided to the City.</u>
<b>Berms, Walls and ROW Planting Requirements</b>				
<b>Berms</b>				
<ul style="list-style-type: none"> <li>• All berms shall have a maximum slope of 33%. Gradual slopes are encouraged. Show 1ft. contours</li> <li>• Berm should be located on lot line except in conflict with utilities.</li> <li>• Berms should be constructed with 6" of topsoil.</li> </ul>				
<b>Residential Adjacent to Non-residential (Zoning Sec 5.5.3.A and LDM 1.a)</b>				
<b>Berm requirements</b> (Zoning Sec 5.5.A)	Adjacent Zoning is RA	No berm is required or provided		
<b>Planting requirements</b> (LDM 1.a.)	LDM Novi Street Tree List	Lines of evergreens are provided at west end of the drive and between lots 8 and 9 and the	Yes	

Item	Required	Proposed	Meets Code	Comments
		residences south of them, and screening trees are also provided between lots 4-7 and the Ballantyne subdivision.		
<b>Adjacent to Public Rights-of-Way (Zoning Sec 5.5.3.A and LDM 1.b)</b>				
<b>Cross-Section of Berms (Zoning Sec 5.5.3.B and LDM 2.j)</b>				
Slope, height and width (Zoning Sec 5.5.3.A.v)	<ul style="list-style-type: none"> <li>Label contour lines</li> <li>Maximum 33% slope</li> <li>Min. 4 feet crest</li> </ul>	Provided	Yes	
Type of Ground Cover		Lawn	Yes	
Setbacks from Utilities	Overhead utility lines and 15 ft. setback from edge of utility or 20 ft. setback from closest pole	Overhead utilities are shown along 8 Mile Road	Yes	
<b>Walls (LDM 2.k &amp; Zoning Sec 5.5.3.vi)</b>				
<b>Material, height and type of construction footing</b>	Freestanding walls should have brick or stone exterior with masonry or concrete interior	A small retaining wall is proposed on Lot 1.		
<b>Walls greater than 3 ½ ft. should be designed and sealed by an Engineer</b>		TBD		<u>Walls taller than 4 feet need to be designed by an engineer</u>
<b>ROW Landscape Screening Requirements (Sec 5.5.3.B. ii)</b>				
<b>Greenbelt width (2)(3) (5)</b>	34 ft./40-foot non-access easement	75 ft	Yes	
Min. berm crest width	4 ft.	<ul style="list-style-type: none"> <li>Berms are proposed on each side of the entry.</li> <li>Crests are 2-5 feet wide</li> </ul>	No	<ol style="list-style-type: none"> <li><u>Please widen the crest of the eastern berm</u></li> <li><u>If possible, please lengthen the western berm to the west.</u></li> </ol>
Minimum berm height (9)	4 ft.	Both berms are approximately 5 feet tall	Yes	
3' wall (4) (7)	NA	No walls are proposed in the greenbelts.	Yes	
<b>Canopy deciduous or large evergreen trees (Sec 5.5.3.B)</b>	<ul style="list-style-type: none"> <li>1 tree per 40 l.f.;</li> <li>8 Mile Road (360-28)/40= 8 trees</li> </ul>	8 trees	Yes	
<b>Sub-canopy deciduous trees (Sec 5.5.3.B)</b>	<ul style="list-style-type: none"> <li>1 tree per 25 l.f.;</li> <li>8 Mile Road (360-28)/25= 13 trees</li> </ul>	18 trees	Yes	

Item	Required	Proposed	Meets Code	Comments
<b>Street Trees</b> (Sec 5.5.3.B and LDM 2)	<u>External Trees</u> <ul style="list-style-type: none"> <li>• 1 tree per 35 l.f.;</li> <li>• 8 Mile Road (360-294)/35= 2 trees or 3 subcanopy trees</li> </ul> <u>Internal streets</u> <ul style="list-style-type: none"> <li>• 1 tree per 35 lf</li> <li>• 1788/35 = 51 trees</li> </ul>	<u>8 Mile Road:</u> 8 subcanopy trees  <u>Internal streets:</u> 51 trees	<ul style="list-style-type: none"> <li>• Yes</li> <li>• Yes</li> </ul>	
<b>Island &amp; Boulevard Planting</b> (Zoning Sec5.5.3.f.ii & LDM 1.d.(1)(e))	<ul style="list-style-type: none"> <li>• Must be landscaped &amp; irrigated</li> <li>• Mix of canopy/sub-canopy trees, shrubs, groundcovers, etc.</li> <li>• No plant materials between heights of 3-6 feet as measured from street grade</li> </ul>	No islands are proposed		
<b>Transformers/Utility boxes</b> (LDM 1.e from 1 through 5)	<ul style="list-style-type: none"> <li>• A minimum of 2ft. separation between box and the plants</li> <li>• Ground cover below 4" is allowed up to pad.</li> <li>• No plant materials within 8 ft. from the doors</li> </ul>	None shown	TBD	<u>When the location of transformer/utility boxes is determined, add landscaping per city requirements.</u>
<b>Detention/Retention Basin Requirements (Sec. 5.5.3.E.iv)</b>				
<b>Planting requirements</b> (Sec. 5.5.3.E.iv)	<ul style="list-style-type: none"> <li>• Clusters of large shall cover 70-75% of the basin rim area at 10 feet from the bottom or permanent water level.</li> <li>• Canopy trees at 1/35 lf measured at 10 feet above the bottom or permanent water level around the east, west and south sides of the basins -woodland replacement trees may be used to meet this requirement.</li> <li>• 10" to 14" tall grass along sides of basin</li> </ul>	An underground detention basin is proposed	NA	
<b>Phragmites and Japanese Knotweed Control</b> (Sec 5.5.6.B.i)	<ul style="list-style-type: none"> <li>• Any and all populations of Phragmites australis and/or Japanese Knotweed on site shall be included on tree</li> </ul>	A note indicates that there is no Phragmites or Japanese knotweed on the site	Yes	



Item	Required	Proposed	Meets Code	Comments
	survey. • Treat populations per MDEQ guidelines and requirements to eradicate the weed from the site.			
<b>LANDSCAPING NOTES, DETAILS AND GENERAL REQUIREMENTS</b>				
<b>Landscape Notes – Utilize City of Novi Standard Notes</b>				
<b>Installation date</b> <i>(LDM 2.l. &amp; Zoning Sec 5.5.5.B)</i>	Intended dates of planting should be between Mar 15 – Nov 15	Mar 15-Nov 15	Yes	
<b>Maintenance &amp; Statement of intent</b> <i>(LDM 2.m &amp; Zoning Sec 5.5.6)</i>	• Include statement of intent to install and guarantee all materials for 2 years. • Include a minimum one cultivation in June, July and August for the 2-year warranty period.	Both notes included	Yes	
<b>Plant source</b> <i>(LDM 2.n &amp; LDM 3.a.(2))</i>	Shall be northern nursery grown, No.1 grade	Yes	Yes	
<b>Irrigation plan</b> <i>(LDM 2.s.)</i>	A fully automatic irrigation system and a method of draining is required with Final Site Plan or alternative means of providing sufficient water for plant establishment and long-term survival	No		1. <u>Need for final site plan</u> 2. <u>The system should meet the requirements listed at the end of this review.</u>
<b>Other information</b> <i>(LDM 2.u)</i>	Required by Planning Commission	NA		
<b>Establishment period</b> <i>(Zoning Sec 5.5.6.B)</i>	<b>2 yr. Guarantee</b>	Yes	Yes	
<b>Approval of substitutions.</b> <i>(Zoning Sec 5.5.5.E)</i>	City must approve any substitutions in writing prior to installation.	Yes	Yes	
<b>Plant List (LDM 4, 11) – Include all cost estimates</b>				
Quantities and sizes	See Table 11.b.(2)(a).i	Show on plant list		
Root type	• B&B or Cont	Show on plant list		
Botanical and common names	• Species native to Michigan shall constitute at least 50% (preferably more) of the plants used, not including woodland replacements or seed	No plant list is provided	TBD	<u>Please provide a plant list no later than the Final Site Plans, preferably on Preliminary Site Plans</u>

Item	Required	Proposed	Meets Code	Comments
	mix species. • Non-woodland replacement trees shall conform to the LDM Section 4 standards for diversity			
Type and amount of lawn		Not shown on plans	TBD	<u>Please clearly indicate which areas are to be seeded with which type of seed on plan view</u>
Cost estimate (LDM 2.t)	For all new plantings, mulch and sod as listed on the plan	No		<u>Need for Final Site Plan</u>
<b>Planting Details/Info (LDM 2.i) – Utilize City of Novi Standard Details</b>				
Canopy Deciduous Tree	Refer to LDM for detail drawings	Yes	Yes	
Evergreen Tree		Yes	Yes	
Multi-stem Tree		Yes	Yes	
Shrub		Yes	Yes	
Perennial/ Ground Cover		Yes	Yes	
Tree stakes and guys. (Wood stakes, fabric guys)		Yes	Yes	
Tree protection fencing	Located at Critical Root Zone (1' outside of dripline)	Yes	Yes	
<b>Other Plant Material Requirements (LDM 3)</b>				
<b>General Conditions</b> (LDM 3.a)	Plant materials shall not be planted within 4 ft. of property line	A callout indicates this	Yes	
<b>Plant Materials &amp; Existing Plant Material</b> (LDM 3.b)	Clearly show trees to be removed and trees to be saved.	<ul style="list-style-type: none"> <li>• All but 2 of the trees on-site will be removed.</li> <li>• Silt fence and tree protection fencing are shown on Sheet 6</li> </ul>	<ul style="list-style-type: none"> <li>• Yes</li> <li>• Yes</li> </ul>	
<b>Landscape tree credit</b> (LDM3.b.(d))	Substitutions to landscape standards for preserved canopy trees outside woodlands/ wetlands should be approved by LA. Refer to Landscape tree Credit Chart in LDM	None		
<b>Plant Sizes for ROW, Woodland replacement and others</b>	Refer to Chapter 37, LDM for more details	No plant list is provided	TBD	<u>Please use correct sizes for plant material on the plant list when it is provided.</u>

Item	Required	Proposed	Meets Code	Comments
(LDM 3.c)				
<b>Plant size credit</b> (LDM3.c.(2))	NA			
<b>Prohibited plants</b> (LDM 11.b(2)b)	No plants on City Invasive Species List	No species are specified	TBD	
<b>Recommended trees for planting under overhead utilities</b> (LDM 3.e)	Label the distance from the overhead utilities			
<b>Collected or Transplanted trees</b> (LDM 3.f)		None		
<b>Nonliving Durable Material: Mulch</b> (LDM 4)	<ul style="list-style-type: none"> <li>• Trees shall be mulched to 3" depth and shrubs, groundcovers to 2" depth</li> <li>• Specify natural color, finely shredded hardwood bark mulch. Include in cost estimate.</li> </ul>	Yes	Yes	

**NOTES:**

1. This table is a working summary chart and not intended to substitute for any Ordinance or City of Novi requirements or standards.
2. The section of the applicable ordinance or standard is indicated in parenthesis. For the landscape requirements, please see the Zoning Ordinance landscape section 5.5 and the Landscape Design Manual for the appropriate items under the applicable zoning classification.
3. Please include a written response to any points requiring clarification or for any corresponding site plan modifications to the City of Novi Planning Department with future submittals.

Irrigation System Requirements

1. Any booster pump installed to connect the project's irrigation system to an existing irrigation system must be downstream of the RPZ.
2. The RPZ must be installed in accordance with the 2015 Michigan Plumbing Code.
3. The RPZ must be installed in accordance with the manufacture installation instructions for winterization that includes drain ports and blowout ports.
4. The RPZ must be installed a minimum of 12-inches above FINISHED grade.
5. Attached is a handout that addresses winterization installation requirements to assist with this.
6. A plumbing permit is required.
7. The assembly must be tested after installation with results recorded on the City of Novi test report form.

WETLAND REVIEW

---

July 15, 2025

Lindsay Bell  
Planner – Community Development  
City of Novi  
45175 Ten Mile Road  
Novi, MI 48375

Submitted electronically to [lbell@cityofnovi.org](mailto:lbell@cityofnovi.org)

Re: Mariella Estates Planned Rezoning Overlay Wetland Review (Formal PRO; JZ24-43)

Dear Lindsay,

Merjent, Inc. (Merjent) has conducted a site plan review of the planned rezoning overlay (PRO) for the Formal PRO Plan for Mariella Estates (site). Two sets of plans were provided:

- One plan prepared by Atwell dated June 27, 2025. This plan contains the primary design/engineering information for the Formal PRO Plan.
- One plan prepared by Allen Design dated June 27, 2025. This plan contains the landscape and woodland replacement information for the Formal PRO Plan.

Merjent reviewed the plans for conformance with the City of Novi’s (City) current Wetlands and Watercourse Protection Ordinance, Chapter 12 Article V. The site is located approximately at 49680 Eight Mile Road in Section 31 of the City (Parcel No. 50-22-31-400-008). The site contains a City-regulated wetland (**Figure 1**).

An Initial Concept Plan review of woodlands was completed for the site on December 3, 2024 and deficiencies were found that required addressing wetland issues at the site. It should be noted that this project was previously identified as “JZ24-43 Preserves of Maybury” and all future correspondence will refer to this project as “Mariella Estates.”

A revised Initial Concept Plan review of wetlands was completed for the site on March 20, 2025 and approval was recommended with requests for minor edits.

### Wetlands

**Wetland Recommendation:** Merjent **recommends approval** of the Mariella Estates Formal PRO **with requests for edits** for future submittals. Additional comments have been provided to meet the City’s Wetlands and Watercourse Protection Ordinance.

Upon review of published resources, the Site appears to contain or immediately borders:

- City-regulated wetlands, as identified on the City of Novi interactive map website. Note that both wetland and property limits depicted on the City’s map are considered approximations (**Figure 1**). Although not depicted on the City of Novi’s interactive regulated wetland map viewer (**Figure 1**), this should be used as an initial planning tool and does not constitute the presence or absence of City-regulated wetlands.

- Wetlands that are regulated by the Michigan Department of Environment, Great Lakes, and Energy (EGLE).
- Wetlands as identified on National Wetland Inventory (NWI) and Michigan Resource Inventory System (MIRIS) maps, as identified on the EGLE Wetlands Viewer interactive map website (map provided in Wetland Boundary Review). NWI and MIRIS wetlands are identified by the associated governmental bodies' interpretation of topographic data and aerial photographs.
- Hydric (wetland) soil as mapped by the U.S. Department of Agriculture, Natural Resources Conservation Service, as identified on the EGLE Wetlands Viewer interactive map website (map provided in Wetland Boundary Review).

**Permits and Regulatory Status**

Due to the comments below, the following wetland-related items may be required for this project:

Item	Required/Not Required
<b>Wetland Permit (specify Non-minor or Minor)</b>	Not Required
<b>Wetland Mitigation</b>	Not Required
<b>Environmental Enhancement Plan</b>	Not Required
<b>Wetland Buffer Authorization</b>	Required
<b>EGLE Wetland Permit</b>	Likely Not Required*
<b>Wetland Conservation Easement</b>	Recommended/Requested

\*Final determination is at the discretion of EGLE

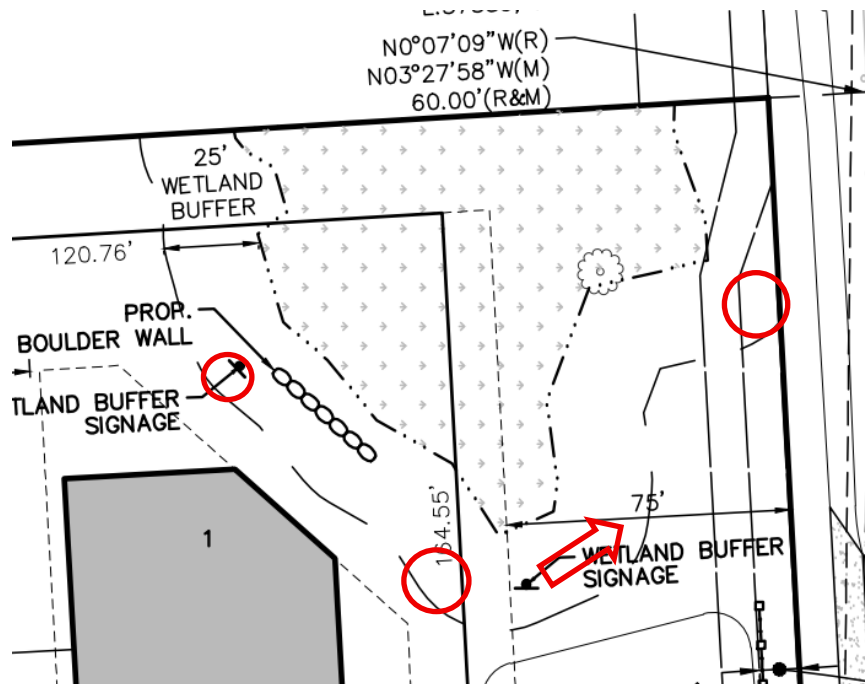
Wetland Review Comments

1. The applicant provided an updated *Wetland Delineation and Threatened and Endangered Species Assessment* dated June 26, 2025. The applicant identified one mixed emergent and forested wetland in the southeast portion of the site. The area is also characterized by a flooded/open water portion. Merjent conducted a site visit on July 1, 2025 and found flagging on-site consistent with the site plan. Photos from the site visit are included as **Attachment A**.
  - a. Due to the presence of multiple wildlife species utilizing the water resource on-site, the on-site water resource should be considered essential to the City of Novi due to meeting the criteria under Section 12-174(b)(6).
2. Pursuant to Section 12-172 (f), *the applicant shall have the boundary lines of any watercourses or wetlands on the property flagged or staked, and the flagging or staking shall remain in place throughout the conduct of the permit activity*. The applicant will need to ensure the area is marked/staked in the field prior to and during construction, if approval is granted for the full extent of the project.
3. The applicant has proposed no impacts to the wetland on-site. Therefore, a wetland permit is not required for this project from the City of Novi. However, due to the proximity of grading and development to the wetlands, the City may request on-site inspections before, during, and/or after construction to ensure water resources are protected pursuant to the site plans.
4. In addition to wetlands, the City of Novi regulates wetland and watercourse buffers/setbacks. Section 3.6(2)(M) of the Zoning Ordinance, Schedule of Regulations, states: "There shall be maintained in all districts a wetland and watercourse setback, as provided herein, unless and to the extent, it is determined to be in the public interest not to maintain such a setback. The intent of this provision is to



require a minimum setback from wetlands and watercourses". The established wetland and watercourse buffer/setback limit is 25 horizontal feet, regardless of grade change. The Applicant should consider modification of the proposed limits of disturbance boundaries in order to preserve wetland and wetland buffer areas. The preservation of the 25-foot wetland buffer areas (also referred to as the "25-foot wetland setback/buffer") is important to the overall health of the wetlands, especially after site development. The existing buffer serves to filter pollutants and nutrients from storm water before entering the wetlands, as well as to provide additional wildlife habitat.

- a. Impacts resulting from the proposed boulder wall should be quantified (cubic yards and square feet) in future submittals.
- b. Impacts from proposed grading should be quantified (cubic yards and square feet) in future submittals. Additionally, if grading is proposed within the wetland setback, it should be identified with a unique symbol.
- c. It is requested that additional wetland and wetland buffer signage be placed around the remaining wetland to ensure the area will not be mowed or disturbed in the future. If possible, the signs should be placed as close to the outside edge of the setback buffer to ensure the full 25-foot setback is not mowed. In future submittals, an example sign rendering should be provided in the site plan. If a conservation easement is placed around the remaining wetland/wetland setback (Comment 4), the signs should indicate that the area is a protected conservation easement.



- d. If possible, any remaining wetland buffer should be seeded with an appropriate native seed mix that follows the criteria set forth in the [City of Novi Landscape Design Manual](#).
5. The Applicant is encouraged to provide wetland conservation easements for any areas of remaining wetland and 25-foot wetland buffer. The Applicant shall provide wetland conservation easements as directed by the City of Novi Community Development Department for any areas of proposed wetland mitigation areas. This language shall be submitted to the City Attorney for review. The executed

---

easement must be returned to the City Attorney within 60 days of the issuance of the City of Novi Wetland and Watercourse permit.

- a. If final approval is granted for this project, it is recommended that the wetland and subsequent remaining buffer on-site be retained in a conservation easement. This will include the prevention of mowing, which could have impacts to the future resident(s) of the proposed Lot 1.

Should you have any questions or concerns with this review, please contact me via email at [jason.demoss@merjent.com](mailto:jason.demoss@merjent.com) or via phone at (619) 944-3835.

Sincerely,

**Merjent, Inc.**



Jason DeMoss, PWS  
Environmental Consultant

Enclosures:

Figure 1 – City of Novi Woodlands and Wetlands Map  
Attachment A – Site Photographs

CC:

Stacey Choi, City of Novi, [schoi@cityofnovi.org](mailto:schoi@cityofnovi.org)

Rick Meader, City of Novi, [rmeader@cityofnovi.org](mailto:rmeader@cityofnovi.org)

Barbara McBeth, City of Novi, [bmcbeth@cityofnovi.org](mailto:bmcbeth@cityofnovi.org)

Matt Pudlo, Merjent, [matt.pudlo@merjent.com](mailto:matt.pudlo@merjent.com)





**Figure 1. City of Novi Regulated Woodlands Map**  
 Approximate Site boundary is shown in Red.

No mapped regulated woodland areas are shown in the map view. Regulated wetland areas are shown in turquoise. Area identified as wetland on-site approximately outlined in blue and filled with yellow.

**Attachment A**  
**Site Photographs**



Flagging around wetland



Overview of wetland on-site



Overview of wetland on-site from the adjacent Ballantyne development

## TRAFFIC REVIEW

---



AECOM  
 39575 Lewis Dr, Ste. 400  
 Novi  
 MI, 48377  
 USA  
 aecom.com

**Project name:**  
 JZ24-43 – Mariella Estates Revised Initial PRO  
 Traffic Review

**From:**  
 AECOM

**Date:**  
 March 20, 2025

**To:**  
 Barbara McBeth, AICP  
 City of Novi  
 45175 10 Mile Road  
 Novi, Michigan 48375

**CC:**  
 Lindsay Bell, Dan Commer, Humna Anjum, Diana  
 Shanahan, Milad Alesmail, Stacey Choi

# Memo

**Subject:** JZ24-43 – Mariella Estates Revised Initial PRO Traffic Review

The revised initial PRO concept site plan was reviewed to the level of detail provided and AECOM recommends **approval** as long as the comments provided below are adequately addressed to the satisfaction of the City.

## GENERAL COMMENTS

1. The applicant, Braciolo Brothers, LLC, is proposing a ten home single-family subdivision.
2. The development is located on the north side of Eight Mile Road, west of Garfield Road. Eight Mile Road is under the jurisdiction of the Road Commission of Oakland County and Garfield Road is under the jurisdiction of the City of Novi.
3. The site is zoned R-A (Residential Acreage) and the applicant is utilizing the PRO option.
4. There are following traffic related deviations are being requested by the applicant:
  - a. Below standard centerline radius.

## TRAFFIC IMPACTS

1. AECOM performed an initial trip generation based on the ITE Trip Generation Manual, 11<sup>th</sup> Edition, as follows.

ITE Code: 210 – Single-Family Detached Housing  
 Development-specific Quantity: 10 Dwelling Units  
 Zoning Change: N/A

Trip Generation Summary	Estimated Trips	Estimated Peak-Direction Trips	City of Novi Threshold	Above Threshold?
<b>AM Peak-Hour Trips</b>	9	7	100	No
<b>PM Peak-Hour Trips</b>	11	7	100	No
<b>Daily (One-Directional) Trips</b>	121	N/A	750	No

2. The City of Novi generally requires a traffic impact study/statement if the number of trips generated by the proposed development exceeds the City's threshold of more than 750 trips per day or 100 trips per either the AM or PM peak hour, or if the project meets other specified criteria.

Trip Impact Study Recommendation	
Type of Study:	Justification

None	-
------	---

## TRAFFIC REVIEW

The following table identifies the aspects of the plan that were reviewed. Items marked O are listed in the City’s Code of Ordinances. Items marked with ZO are listed in the City’s Zoning Ordinance. Items marked with ADA are listed in the Americans with Disabilities Act. Items marked with MMUTCD are listed in the Michigan Manual on Uniform Traffic Control Devices.

The values in the ‘Compliance’ column read as ‘met’ for plan provision meeting the standard it refers to, ‘not met’ stands for provision not meeting the standard and ‘inconclusive’ indicates applicant to provide data or information for review and ‘NA’ stands for not applicable for subject Project. The ‘remarks’ column covers any comments reviewer has and/or ‘requested/required variance’ and ‘potential variance’. A potential variance indicates a variance that will be required if modifications are not made or further information provided to show compliance with the standards and ordinances. The applicant should put effort into complying with the standards; the variances should be the last resort after all avenues for complying have been exhausted. Indication of a potential variance does not imply support unless explicitly stated.

EXTERNAL SITE ACCESS AND OPERATIONS				
No.	Item	Proposed	Compliance	Remarks
1	Driveway Radii   O <a href="#">Figure IX.3</a>	35'	Met	
2	Driveway Width   O <a href="#">Figure IX.3</a>	28'	Met	
3	Driveway Taper   O <a href="#">Figure IX.11</a>			
3a	Taper length	75' and 100'	Met	
3b	Tangent	0' and 50'	Met	
4	Emergency Access   O <a href="#">11-194.a.19</a>	-	N/A	
5	Driveway sight distance   O <a href="#">Figure VIII-E</a>	610'	Met	
6	Driveway spacing			
6a	Same-side   O <a href="#">11.216.d.1.d</a>	400.83' and 1084.63'	Met	
6b	Opposite side   O <a href="#">11.216.d.1.e</a>	200.87' to private drive	Met	
7	External coordination (Road agency)	Indicated coordination with RCOC	Met	
8	External Sidewalk   <a href="#">Master Plan &amp; EDM</a>	8' proposed, tying into existing	Met	
9	Sidewalk Ramps   <a href="#">EDM 7.4 &amp; R-28-K</a>	Indicated	<b>Partially Met</b>	<b>Update detail to latest R-28-K version in future submittal.</b>
10	Any Other Comments:			

INTERNAL SITE OPERATIONS				
No.	Item	Proposed	Compliance	Remarks
11	Loading zone   <a href="#">ZO 5.4</a>	-	N/A	

INTERNAL SITE OPERATIONS				
No.	Item	Proposed	Compliance	Remarks
12	Trash receptacle   <a href="#">ZO 5.4.4</a>	Not indicated	Inconclusive	Indicate if individual trash collection is to be present for single family homes.
13	Emergency Vehicle Access	Turning movements not provided	Inconclusive	Provide turning movements in future submittal.
14	Maneuvering Lane   <a href="#">ZO 5.3.2</a>	-	N/A	
15	End islands   <a href="#">ZO 5.3.12</a>			
15a	Adjacent to a travel way	-	N/A	
15b	Internal to parking bays	-	N/A	
16	Parking spaces   <a href="#">ZO 5.2.12</a>	On-street parking not allowed	Met	
17	Adjacent parking spaces   <a href="#">ZO 5.5.3.C.ii.i</a>	-	N/A	
18	Parking space length   <a href="#">ZO 5.3.2</a>	-	N/A	
19	Parking space Width   <a href="#">ZO 5.3.2</a>	-	N/A	
20	Parking space front curb height   <a href="#">ZO 5.3.2</a>	-	N/A	
21	Accessible parking – number   <a href="#">ADA</a>	-	N/A	
22	Accessible parking – size   <a href="#">ADA</a>	-	N/A	
23	Number of Van-accessible space   <a href="#">ADA</a>	-	N/A	
24	Bicycle parking			
24a	Requirement   <a href="#">ZO 5.16.1</a>	-	N/A	
24b	Location   <a href="#">ZO 5.16.1</a>	-	N/A	
24c	Clear path from Street   <a href="#">ZO 5.16.1</a>	-	N/A	
24d	Height of rack   <a href="#">ZO 5.16.5.B</a>	-	N/A	
24e	Other (Covered / Layout)   <a href="#">ZO 5.16.1</a>	-	N/A	
25	Sidewalk – min 5' wide   <a href="#">Master Plan</a>	5' proposed	Met	
26	Sidewalk ramps   <a href="#">EDM 7.4</a> & <a href="#">R-28-K</a>	None proposed	N/A	
27	Sidewalk – distance back of curb   <a href="#">EDM 7.4</a>	10' proposed	Met	
28	Cul-De-Sac   <a href="#">O Figure VIII-F</a>	-	N/A	
29	EyeBrow   <a href="#">O Figure VIII-G</a>	54' radius proposed	Met	
30	Turnaround   <a href="#">ZO 5.10</a>	Proposed and dimensioned	Met	
31	Any Other Comments:	<b>Proposed 90' centerline radius is below 230' standard per Section 11-194.b.2 of the City's Code of Ordinances. The applicant is requesting a deviation.</b>		

SIGNING AND STRIPING				
No.	Item	Proposed	Compliance	Remarks
32	Signing: Sizes   <a href="#">MMUTCD</a>	Indicated	Met	
33	Signing table: quantities and sizes	Indicated	Met	Remove "typ." from the sign labels on site plan.



SIGNING AND STRIPING				
No.	Item	Proposed	Compliance	Remarks
34	Signs 12" x 18" or smaller in size shall be mounted on a galvanized 2 lb. U-channel post   <a href="#">MMUTCD</a>	Not indicated	<b>Inconclusive</b>	Provide in future submittal.
35	Signs greater than 12" x 18" shall be mounted on a galvanized 3 lb. or greater U-channel post   <a href="#">MMUTCD</a>	Not indicated	<b>Inconclusive</b>	Provide in future submittal.
36	Sign bottom height of 7' from final grade   <a href="#">MMUTCD</a>	Not indicated	<b>Inconclusive</b>	Provide in future submittal.
37	Signing shall be placed 2' from the face of the curb or edge of the nearest sidewalk to the near edge of the sign   <a href="#">MMUTCD</a>	Not indicated	<b>Inconclusive</b>	Provide in future submittal.
38	FHWA Standard Alphabet series used for all sign language   <a href="#">MMUTCD</a>	Not indicated	<b>Inconclusive</b>	Provide in future submittal.
39	High-Intensity Prismatic (HIP) sheeting to meet FHWA retro-reflectivity   <a href="#">MMUTCD</a>	Not indicated	<b>Inconclusive</b>	Provide in future submittal.
40	Parking space striping notes	-	N/A	
41	The international symbol for accessibility pavement markings   ADA	-	N/A	
42	Crosswalk pavement marking detail	-	N/A	
43	Any Other Comments:	The applicant is proposing flashing beacon signs on 8 Mile Road that will be installed by RCOC.		

Note: Hyperlinks to the standards and Ordinances are for reference purposes only, the applicant and City of Novi to ensure referring to the latest standards and Ordinances in its entirety.


Should the City or applicant have questions regarding this review, they should contact AECOM for further clarification.

Sincerely,

**AECOM**



Paula K. Johnson, PE  
Senior Transportation Engineer



Saumil Shah  
Project Manager

## FIRE REVIEW

---



November 14, 2024

TO: Barbara McBeth - City Planner  
Lindsay Bell - Plan Review Center  
Dan Commer – Plan Review Center  
Diana Shanahan – Plan Review Center

RE: Preserves of Maybury – Initial PRO

**CITY COUNCIL**

**Mayor**  
Justin Fischer

**Mayor Pro Tem**  
Laura Marie Casey

Dave Staudt

Brian Smith

Ericka Thomas

Matt Heintz

Priya Gurumurthy

**City Manager**  
Victor Cardenas

**Director of Public Safety**  
**Chief of Police**  
Erick W. Zinser

**Fire Chief**  
John B. Martin

**Assistant Chief of Police**  
Scott R. Baetens

**Assistant Fire Chief**  
Todd Seog

**JZ24-43**

**Project Description:** Build new subdivision on 9.36 acres, with 10 Single family homes.

**Comments:**

- **All fire hydrants MUST be installed and operational prior to any combustible material is brought on site. IFC 2015 3312.1. ONE additional hydrant is needed near lot #1. Sheet #5 only shows 2 hydrants being added to new water main.**
- Fire lanes will be designated by the Fire Chief or his designee when it is deemed necessary and shall comply with the Fire Prevention Ordinances adopted by the City of Novi. The location of all “fire lane – no parking” signs are to be shown on the site plans. **(Fire Prevention Ord.). Additional “No Parking signs” needed at end of proposed Maybury Dr, near cul-de-sac turnaround.**
- Fire apparatus access drives to and from buildings through parking lots shall have a minimum fifty (50) feet outside turning radius and designed to support a minimum of thirty-five (35) tons. **(D.C.S. Sec 11-239(b)(5)). Sheet #5 indicates 45’ turning radii. Have this updated and include 50’ turning capabilities.**
- **All other Fire Department notes (from sheet 1) will be followed for next review.**

**Recommendation:**

Approved w/Conditions to be followed from list above for next review.

Sincerely,

Andrew Copeland – Acting Fire Marshal  
City of Novi Fire Department  
cc: file

Novi Public Safety Administration  
45125 Ten Mile Road  
Novi, Michigan 48375  
248.348.7100  
248.347.0590 fax

cityofnovi.org

APPLICANT RESPONSE LETTERS

---



August 8, 2025

Lindsay Bell, AICP  
Senior Planner  
45175 Ten Mile Road  
Novi, MI 48103

RE: *JZ24-43 Mariella Estates PRO*

Ms. Bell,

Thank you for providing your review and feedback for the above referenced project. We have revised the plans in accordance with your review letter dated July 18, 2025. For your use, below are our responses on how we have addressed or plan to address each of the comments in your letter.

### **General Comments from the Review Letter – reviewed by Lindsay Bell**

---

1. The applicant has provided a wetland delineation and endangered species assessment prepared by Atwell, dated June 26, 2025. See the Wetland Review for detailed comments on these documents. Further, the applicant has proposed a boulder wall within the wetland buffer on Lot 1, as well as wetland buffer signage in two locations to discourage disturbance of the buffer. Both the boulder wall and the signage should be relocated to be at the edge of the 25-foot buffer to more effectively delineate the buffer area. Additional signs should also be placed, and the buffer shall be planted with a native seed mix as mentioned in the Wetland Review. Staff suggests a conservation easement over the wetland area as an additional benefit in the interest of the general public.

**Response:** *The lot line for lot 1 has been modified so that the wetland is not within the proposed lot. With the next submittal the boulder wall and signage (including additional signage) will be moved to the edge of the buffer. A conservation easement will be proposed over the wetland.*

2. The City also prefers to keep 25-foot wetland buffers out of private lot boundaries to better protect them from disturbance. The applicant will need to adjust the Lot 1 boundaries to exclude the delineated wetland, which would not cause new deviations for lot width, but may affect the lot area. The applicant shall calculate the lot area of the redrawn Lot 1 to determine if any additional deviations need to be requested, or other lot adjustments need to be made.

**Response:** *Lot one has been modified to stay out of the wetland boundary. While the wetland setback still exists on the lot this will be within a proposed conservation easement and the buildable area of the lot will not include any buffer.*

3. No architectural drawings have been provided.

**Response:** *Architectural drawings will be provided during final site plan.*

4. The applicant should consider sustainable, energy-efficient and best-practice design for site elements and building materials, such as LEED recommended strategies.

**Response:** *Noted, the applicant will pursue sustainable design features where practical.*

5. The applicant should consider placing the wetland in a conservation easement, which would be considered an additional enhancement to the project.

**Response:** *A conservation easement will be provided over the existing wetland.*

6. Both the signage and the wall should be relocated to the outer edge of the 25-foot buffer to more effectively limit disturbance, including mowing, cutting, planting and removal of vegetation. The number of signs should also be increased to allow placement at 50-foot intervals.

**Response:** *Noted, these revisions will be made with the next submittal.*

7. This is a PRO in which the applicant seeks both a rezoning and a list of ordinance deviations. In Staff's opinion the conditions could be further enhanced to result in a positive impact to the public, and we have offered some suggestions for the applicant to consider in this and the other review letters.

**Response:** *The applicant and their consultant feel that given the scale of the development the proposed public benefit is appropriate.*

#### **Planning – reviewed by Lindsay Bell**

---

1. Lots 4 and 5 require a deviation for lot width.

**Response:** *This and all deviations are noted on the cover.*

2. Lot 1 shall include buffer delineation to prevent encroachment/mowing/removal of vegetation.

**Response:** *The wetland and buffer will be placed in a conservation easement.*

3. See Engineering Review letter for detail on SWM Plan comments.

**Response:** *No additional engineering review letter was provided in this review. All comments in the previous engineering review letter have either been addressed or will be addressed during the PSP or FSP.*

4. The committee has considered and approved the requested names.

**Response:** *Noted.*

#### **Landscape – reviewed by Rick Meader**

---

1. Please provide a current wetland delineation.

**Response:** *A current wetland delineation has been provided.*

2. See the Merjent review for a complete discussion of the trees and wetlands.

**Response:** *Noted.*

3. Will trees #2401 and #2402 be removed? It appears they are within the wetland that is not being changed.

**Response:** *These trees are no longer proposed to be removed.*

#### **Wetlands – reviewed by Jason DeMoss**

---

1. The applicant provided an updated Wetland Delineation and Threatened and Endangered Species Assessment dated June 26, 2025. The applicant identified one mixed emergent and forested wetland in the southeast portion of the site. The area is also characterized by a flooded/open water portion. Merjent conducted a site visit on July 1, 2025 and found flagging on-site consistent with the site plan. Photos from the site visit are included as Attachment A.

- a. Due to the presence of multiple wildlife species utilizing the water resource on-site, the on-site water resource should be considered essential to the City of Novi due to meeting the criteria under Section 12-174(b)(6).

**Response:** *Noted.*

2. Pursuant to Section 12-172 (f), the applicant shall have the boundary lines of any watercourses or wetlands on the property flagged or staked, and the flagging or staking shall remain in place throughout the conduct of the permit activity. The applicant will need to ensure the area is marked/staked in the field prior to and during construction, if approval is granted for the full extent of the project.

**Response:** *Understood, the wetlands will be flagged during construction activity.*

3. The applicant has proposed no impacts to the wetland on-site. Therefore, a wetland permit is not required for this project from the City of Novi. However, due to the proximity of grading and development to the wetlands, the City may request on-site inspections before, during, and/or after construction to ensure water resources are protected pursuant to the site plans.

**Response:** *Noted.*

4. In addition to wetlands, the City of Novi regulates wetland and watercourse buffers/setbacks. Section 3.6(2)(M) of the Zoning Ordinance, Schedule of Regulations, states: "There shall be maintained in all districts a wetland and watercourse setback, as provided herein, unless and to the extent, it is determined to be in the public interest not to maintain such a setback. The intent of this provision is to require a minimum setback from wetlands and watercourses". The established wetland and watercourse buffer/setback limit is 25 horizontal feet, regardless of grade change. The Applicant should consider modification of the proposed limits of disturbance boundaries in order to preserve wetland and wetland buffer areas. The preservation of the 25-foot wetland buffer areas (also referred to as the "25- foot wetland setback/buffer) is important to the overall health of the wetlands, especially after site development. The existing buffer serves to filter pollutants and nutrients from storm water before entering the wetlands, as well as to provide additional wildlife habitat.

- a. Impacts resulting from the proposed boulder wall should be quantified (cubic yards and square feet) in future submittals.
- b. Impacts from proposed grading should be quantified (cubic yards and square feet) in future submittals. Additionally, if grading is proposed within the wetland setback, it should be identified with a unique symbol.
- c. It is requested that additional wetland and wetland buffer signage be placed around the remaining wetland to ensure the area will not be mowed or disturbed in the future. If possible, the signs should be placed as close to the outside edge of the setback buffer to ensure the full 25-foot setback is not mowed. In future submittals, an example sign rendering should be provided in the site plan. If a conservation easement is placed around the remaining wetland/wetland setback (Comment 4), the signs should indicate that the area is a protected conservation easement.
- d. If possible, any remaining wetland buffer should be seeded with an appropriate native seed mix that follows the criteria set forth in the City of Novi Landscape Design Manual.

**Response:** *Noted, the above request will be addressed in a future submittal.*

5. The Applicant is encouraged to provide wetland conservation easements for any areas of remaining wetland and 25-foot wetland buffer. The Applicant shall provide wetland conservation easements as directed by the City of Novi Community Development Department for any areas of proposed wetland mitigation areas. This language shall be submitted to the City Attorney for review. The executed easement must be returned to the City Attorney within 60 days of the issuance of the City of Novi Wetland and Watercourse permit.

- a. If final approval is granted for this project, it is recommended that the wetland and subsequent remaining buffer on-site be retained in a conservation easement. This will include the prevention of mowing, which could have impacts to the future resident(s) of the proposed Lot 1.

**Response:** *Understood, a conservation easement will be placed over the wetland and buffer prior to construction.*

We appreciate your continued review and assistance with this project. Should you have any remaining questions or need anything else from us to help facilitate your approvals, please do not hesitate to contact me direct at (947)-886-9874.

Sincerely,  
**ATWELL, LLC**



Christopher J. Rothhaar, P.E.  
Land Development- Project Manager



**PLANNING COMMISSION MINUTES - EXCERPT**  
**April 23, 2025**

---



# PLANNING COMMISSION MINUTES

CITY OF NOVI  
Regular Meeting

**April 23, 2025 7:00 PM**

Council Chambers | Novi Civic Center  
45175 Ten Mile Road, Novi, MI 48375 (248) 347-0475

## CALL TO ORDER

The meeting was called to order at 7:00 PM.

## ROLL CALL

Present: Chair Pehrson, Member Lynch, Member Becker, Member Roney, Member Verma

Absent Excused: Member Avdoulos, Member Dismondy

Staff: Barbara McBeth, City Planner; Thomas Schultz, City Attorney; Lindsay Bell, Senior Planner; Diana Shanahan, Staff Planner; Rick Meader, Landscape Architect; Humna Anjum, Project Engineer

## PLEDGE OF ALLEGIANCE

Member Lynch led the meeting attendees in the recitation of the Pledge of Allegiance.

## APPROVAL OF AGENDA

Motion made by Member Lynch and seconded by Member Verma to approve the April 23, 2025 Planning Commission Agenda.

**VOICE VOTE ON MOTION TO APPROVE THE APRIL 23,2025 PLANNING COMMISSION AGENDA MOVED BY MEMBER LYNCH AND SECONDED BY MEMBER VERMA. *Motion carried 5-0.***

## AUDIENCE PARTICIPATION

Chair Pehrson invited members of the audience who wished to address the Planning Commission during the first audience participation to come forward. Seeing no one, Chair Pehrson closed the first public audience participation.

## CORRESPONDENCE

There was not any correspondence.

## COMMITTEE REPORTS

There were no Committee reports.

## CITY PLANNER REPORT

There was no City Planner report.

## CONSENT AGENDA - REMOVALS AND APPROVALS

There were no consent agenda removals or approvals.

## PUBLIC HEARINGS

1. **JZ24-43 MARIELLA ESTATES PRO PLAN WITH REZONING 18.750**

Public hearing at the request of Braciolo Brothers, LLC for initial submittal and eligibility discussion for a Zoning Map Amendment from RA Residential Acreage to R-1 One-Family Residential with a Planned Rezoning Overlay. The subject site is approximately 9.4 acres and is located west of Garfield Road, on the north side of Eight Mile Road (Section 31). The applicant is proposing to develop 10 single family lots.

Senior Planner Lindsay Bell stated that the applicant is proposing to rezone about 9.4 acres north of Eight Mile Road, west of Garfield Road, utilizing the Planned Rezoning Overlay option. The surrounding development to the north, west and east is newer single family developments. There are also single family lots bordering on the southwest, and south of 8 Mile Road is Maybury State Park.

The current zoning of the property is Residential Acreage. The properties to the north, east and west are also zoned RA, but have developed under the Residential Unit Development option, or RUD. The Future Land Use Map identifies this property and those around it as Single Family. The density map shows a maximum planned density of 0.8 dwellings per acre.

The natural features map does not show any regulated features on the property, however current and historic aerial photos show a pond feature in the southeast corner of the property. We have asked the applicant to provide additional information to be able to determine if it is a regulated wetland. The tree survey also indicates trees that are greater than 36-inches in diameter, which are regulated by the woodland ordinance.

Lindsay Bell stated the applicant had wanted to use the RUD option, which is how the adjacent Ballentyne and Parc Vista developments were approved, however that option requires a minimum site size of 20 acres. Therefore, they have proposed utilizing the Planned Rezoning Overlay to rezone the property to R-1 One Family Residential to achieve a similar development. The initial concept plan shows 10 single family lots. The development is accessed by a private gated street with one entrance off Eight Mile Road. While not required in the R-1 district, they have proposed a 20-foot landscape buffer around the lots to make the development more consistent with the Ballentyne and Parc Vista developments.

The proposed Mariella Estates would have very similar minimum and average lot sizes to the surrounding developments, with the smallest lots being one-half acre and the largest being almost three-quarters of an acre. No façade elevations have been provided, but the applicant indicates these will be custom-built homes that would need to comply with ordinance standards at the time of plot plan review for individual lots.

Rezoning to the R-1 category requested by the applicant would permit the use proposed. Some of the conditions proposed include:

1. Open space as shown on the plan. Originally the applicant was proposing as a benefit to have a play structure within the park that would be available to the public. However, staff noted that because the entrance to the development is proposed to be gated, it would be unlikely that non-residents would end up using the park. There are also nearby public parks, such as ITC Park and Maybury State Park, which makes a park at this location less of a priority.
2. Perimeter landscape buffers that offer additional separation from the existing lots. The applicant states additional trees will be provided in the buffers in future submittals.
3. Limiting the overall density of the development to 1.07, which is more restrictive than the 1.6 dwellings per acre permitted in the R-1 District and more similar to the 0.8 dwellings per acre average of the surrounding developments.
4. Upgrading the crosswalk at on 8 Mile - The existing crosswalk is approximately 460 feet east of the entrance to the proposed development, and there is no paved pathway on the south side of Eight Mile Road to get users of the crosswalk to the entrance of Maybury State Park. The applicant states in their response letter that they will pursue providing a 5-foot pathway that avoids the ditch and natural features on the south side of Eight Mile. Staff has advised consideration be given to relocate the crosswalk westward, closer to the entrance of Maybury State Park, while also

upgrading the crosswalk with flashing signage if a crosswalk study indicates this treatment is warranted.

5. The applicant also now shows the pond area in the southeast corner to be preserved.

Staff and consultants have not identified any significant issues with the proposed rezoning and Concept Plan. There are only three deviations requested, which staff support as they are each relatively minor. No deviations for building height or setbacks are proposed.

Planning Commission will not make a formal recommendation to City Council at this meeting. Instead, the first public hearing is an opportunity for the members of the Planning Commission to hear public comment, and to review and comment on whether the project meets the requirements of eligibility for Planned Rezoning Overlay proposal. Following the Planning Commission public hearing, the project would then go to City Council for its review and comment on the eligibility.

After this initial round of comments by the public bodies, the applicant may choose to make any changes, additions or deletions to the proposal based on the feedback received. The subsequent submittal would then be reviewed by City staff and consultants, and then the project would be scheduled for a second public hearing before Planning Commission. Following the second public hearing, the Planning Commission would make a recommendation for approval or denial to City Council.

Tonight, the Planning Commission is asked to hold the public hearing, and to review and comment on the proposed rezoning. The applicant Antonello Stante from Braciolo Brothers, as well as engineer Matt Bush from Atwell, are here representing the project. Staff is available to answer any questions you may have.

Chair Pehrson invited the applicant to address the Planning Commission.

Mr. Matthew Bush with Atwell thanked the Planning Commission for the opportunity to present Mariella Estates. He introduced Mr. Antonello Stante from Braciolo Brothers.

Mr. Antonello Stante stated he is developing this project in partnership with his family. He shared he has been working within the Novi community since 1979 and currently lives at Eight Mile and Beck Road. He expressed appreciation to the Planning Commission for their consideration.

Mr. Matthew Bush stated the project is located at Eight Mile and Garfield Road. He noted the properties flanking the subject property were developed under the RUD development option. Due to the ordinance requirement of 20 acres for the RUD option they are respectfully proposing a PRO with R-1.

The proposed project consists of ten single-family luxury homes for sale with a half-acre minimum lot size. He noted twenty-eight percent of the site consists of open space. Included in the plan are perimeter landscape buffers, frontage public road buffers, large active open space park, and a pedestrian node with bench seating.

Mr. Bush stated several options for the proposed public benefit as part of the PRO were explored. He expressed they are open to feedback from Commissioners and Staff. The public benefit being proposed at this time is a safety enhancement to the existing crosswalk. This enhancement includes two rectangular rapid beacon signs on either side of the crosswalk to provide safer access to Maybury State Park. Mr. Bush expressed that relocating the crosswalk closer to Maybury State Park was also discussed.

Mr. Bush shared a slide showing several custom homes built by Braciolo Brothers to give an idea of Mr. Stante's vision and noted this is a well-balanced proposal that is consistent with the surrounding land development. He stated they are open to feedback and thanked the Planning Commission for their consideration.

Chair Pehrson opened the public hearing and invited members of the audience who wished to speak to approach the podium. Seeing no one, Chair Pehrson requested Member Lynch read into the record the correspondence received. Member Lynch relayed correspondence was received from Mr. Zhou who objects due to the addition of traffic, construction noise, safety concerns, and property value. Sana Syed and Navin Raj object due to concerns of stormwater runoff, small lot sizes, and privacy. Dr. Danielle Zazaian objects due to increased traffic, environmental impacts, and loss of neighborhood character. Diana Sanchez objects due to concerns regarding the spacing of houses in the proposed development and the public park. Giridhar Pothula objects due to rear setbacks and lack of recreation space. Christina Calo supports and expresses it will be a great addition to the community.

Mr. Zhou at 21077 Ballantyne Boulevard inquired if the developer would develop the maximum number of seven lots under the current zoning if the PRO is not approved.

Chair Pehrson closed the public hearing and turned the matter over to the Planning Commission for consideration.

Member Lynch expressed familiarity with Mr. Stante's other project and from what he has seen the proposed development will be in keeping with or a little higher level than the homes in adjacent communities. He stated the water runoff has been addressed and will be contained on site. He noted property values will remain unchanged or be improved. Overall, the proposed development will fit with the character of the surrounding developments with lot sizes being substantially similar to the adjacent lot sizes.

Member Lynch stated he is not in favor of the proposed public park as it is in the gated community. Regarding the public benefit as part of the PRO he expressed that this portion is a City Council decision. He noted he does not think it is a good idea to put the crosswalk directly across from the main entrance and stated the crosswalk leading to the trail is a better idea. Additionally, he advised further consideration should be given to the landscape plan in the future.

Member Lynch shared he had driven through Mr. Stante's other development, noting the architecture is beautiful and unique.

Mr. Stante stated the proposed development will only be ten homes and will not be crowded; he is aware people will be looking for privacy.

Member Lynch stated he believes the proposal will fit into the area. He noted the landscaping plan could show additional landscaping in the future and other options for the public benefit should be explored. Overall, he believes the project will fit into the area.

Member Becker inquired if the parallel plan from the packet will be commented on.

Senior Planner Bell stated the parallel plan is an example of what could be developed under an RA development fitting in the requirement for a proper road and stormwater.

Member Becker stated that the biggest single thing that is being looked at is the PRO. The PRO must constitute an overall benefit to the public and outweigh any detriment that otherwise could not be accomplished without the proposed rezoning. There must be justification for amending an existing zoning ordinance, and this should be done very carefully. He expressed he does not see an overall public benefit that outweighs any detriment. He noted that is something the applicant can work on, otherwise he believes it does not meet the test for a PRO. Additionally, Member Becker stated he noticed that the park/open space covers the stormwater vault system and expressed that it was a brilliant idea. Finally, he stated moving the crosswalk to the west would make the crosswalk less safe.

Member Verma inquired whether more trees are being considered for the landscape buffer.

Mr. Stante confirmed that additional trees will be considered.

Member Roney relayed that the question being considered is if this qualifies for the PRO. He stated he thinks it does qualify, but there are items that must be addressed. The landscaping should be considered further, he noted the applicant has said the landscaping will be addressed in a future submittal. Secondly, the public benefit aspect of the PRO should be looked at. He stated the five-foot pathway that was seen in the packet is a better way for people to cross to Maybury State Park. Lastly, regarding the objections to the size of the lots, the lot sizes are approximately half-acre as shown on the slide. He stated the concept plan needs to be fine-tuned.

Chair Pehrson stated that he would like the applicant to address the construction element regarding how the construction will be approached as to not disrupt the other neighbors. He stated there is not an issue with traffic relative to either RA or the PRO and is in agreement with the other commissioners that the crosswalk should not be moved further to the west. Moving the crosswalk would create a problem that doesn't exist. In a future submittal he would be looking for the crosswalk to remain in its current location. Regarding the public benefit, it was stated that not enough has been heard to provide direction that this satisfies the PRO. He stated this satisfies everything relative to the RA and the subject of the PRO needs to be addressed in a much more aggressive manner.

**This agenda item was discussed, but a motion on the item was not required.**

**2. JSP24-31 DICK'S SPORTING GOODS – HOUSE OF SPORT**

Public hearing at the request of Dick's Sporting Goods for Planning Commission's recommendation of a Special Land Use Permit and Preliminary Site Plan. The subject property at 27600 Novi Road totals approximately 17.79 acres and is located east of Novi Road, south of Twelve Mile Road (Section 14). The property is zoned R-C (Regional Center District). The applicant is proposing to occupy a portion of the existing 241,725 square foot building and construct an outdoor track/field area adjacent to the building.

Planner Diana Shanahan stated the 17.79-acre parcel is part of the Twelve Oaks Mall located on the east side of Novi Road, south of Twelve Mile Road in section 14 of the city. The site and surrounding area are zoned RC: Regional Commercial District. The Future Land Use map indicates Regional Commercial for this property as well as for the surrounding properties. The subject property does not contain regulated natural features.

Dick's House of Sport plans to occupy the majority of the lower level of the former Sears building, with some space removed for a shared loading dock, and vestibules carved out for upper-level tenant access. The partial renovation of the building includes the demolition of the Sears auto center and modification to the northwest corner of the building to create a 2-story open area for a climbing wall. Future TBD tenants will occupy the upper level.

An outdoor activity space with a turf field and running track, enclosed by a 40' fence and accessed from inside the store, will be constructed in the exterior area of the demolished auto center. The outdoor activity space will provide versatile use for product testing, open play, rental use, and specialty events. In the winter months the outdoor space will have ice rink capability. A chiller is proposed on the northeast side of the track and field to convert the outside space into an ice rink in the winter. Access to the track/field area for a Zamboni is proposed at the southwest corner of the fenced perimeter. During the ice rink season, the Zamboni will be stored inside the southwest corner of the track/field area, under a covered shelter. When the ice rink is not in operation, the Zamboni will be stored off-site.

Overall façade changes include new exterior entries along the west side of the building, the main 2-story entry, and the north side, a secondary 1-story entry to access the track/field from the store.

**CITY COUNCIL MINUTES - EXCERPT**  
**May 19, 2025**

---

**Roll call vote on CM 25-05-67**

**Yeas: Smith, Staudt, Thomas, Fischer, Casey,  
Gurumurthy, Heintz**

**Nays: None**

**3. Initial review of eligibility of Mariella Estates, JZ25-43, to rezone property north of Eight Mile Road, west of Garfield Road, from Residential Acreage to R-1 One Family with a Planned Rezoning Overlay.**

City Manager Cardenas said this is just for the Council's initial reaction and a question-and-answer period with the developer of the proposal before they go through the formal PRO process with the planning decision then ultimately coming back for the Council final decision. He said this is the rezoning of 9.4 acres on 8 Mile Road to allow the development of 10 single family houses with half acre lots. He said this will be in the R-1 district under the current zoning, which currently allows for six single family lots. He said the applicant is proposing the public benefit of a five-foot wide paved pathway from the existing 8 Mile Road crosswalk to the entry of Maybury State Park, approximately about 280 feet. He said Maybury will have to approve this as well and they are still in the negotiation process. He said additionally the applicant would like to include a contribution to improvements at the City's ITC Park in the amount of \$15,000. He said specific improvements would be identified by a formal PRO agreement. However, the applicant has talked about baseball dugout covers and bench seating, a new playground structure, walking paths, and additional sporting infrastructure.

Matthew Bush, Atwell, on behalf of Mr. Antonello Stante, said he is there to present the Mariella Estates residential development. Antonello Stante said they have been doing site work in Novi since 1978. He said they did another development in Northville, and they are trying to recreate the same thing here in Novi. He said they would like to build more custom homes here in Novi and try to recreate the Bella Vista site in Northville. Mr. Bush said the site is a 9.3 acre site located on the north side of 8 Mile Road just west of Garfield. He said you will notice Ballentyne and Parc Vista also under construction and are all RUD projects with a minimum lot sizes of half an acre. He said their site does not meet ordinance requirements due to the size to be a RUD or else they would do an RUD to be consistent with the area. He said in order to be consistent with the nearby developments, that is the reason they are offering a PRO to allow them to do the half acre lot sizes. He said they plan to do 10 single family luxury custom homes with half acre minimum lot sizes. He said their density is just over one house per acre, which is significantly below the R-1 zoning, which led them to the PRO process. He said there are a couple of highlights to the plan like open space. He said there will be underground detention to allow for a usable park area near the entrance. He said they will have a significant amount of open space still providing for landscape buffers around the perimeter, and they are still talking to the Planning Commission. He said they are amenable to really beefing up the landscaping around setbacks. He said assuming this does move forward to site planning that will be something they will incorporate into the plan. He reiterated that there will be a large open park and a central node for mail kiosks, and a gathering point for residents. He said when they originally went to planning commission they proposed some of the pedestrian rapid flashing beacon signs for the pathway crossing, but after discussion with



the commission it was determined that was not as significant of a public benefit as they want as well as traffic concerns on 8 Mile Road. He said they went back and revised the public benefit that they feel is significant and well scaled to the size of their project, and are looking, pending approval, to pave a pathway from the crosswalk out to Maybury State Park. He said also nearby ITC Park they are looking to do some improvements to the baseball dugouts or potentially funding some of the concept planning next door, but it is under further discussion. He gave photographic examples of some of the homes they have previously built for the Bella Vista project in Northville and said that it is relative to the vision they have for the more custom diverse look of the homes and the luxury residents will feel.

Member Smith asked if there was a safety reason that the sidewalk along 8 Mile Road doesn't currently go down to the Maybury Park entrance. He said that creating a 280-foot sidewalk will lead to nowhere and the City will have to take care of it. He said that the City already has a sidewalk on the north side. Mr. Bush said that the idea was to have a pathway across the road, and that there is a seemingly clear-cut tree area that goes out to Maybury Park for intentional use but right now it is just gravel and dirt. He said the idea for the public benefit is to pave the path out to Maybury Park. He said they cannot run it along the road in the right-of-way because there are a lot of trees right up to the roadside ditch. He said the proposal is to pave the existing clear-cut path that goes out to Maybury Park. Member Smith said he would rather see the pathway cross where the regular park entrance is if that is safe and possible to do so. He said extra sidewalk to take care of isn't something the City is really interested in doing, he doesn't think. He asked if the public playground would be behind a gate, or if it would be accessible, and if this was a gated community. Mr. Bush said that was an original consideration, but they would open up the smaller area park to the public. He said they decided that the vision was to gate the development and that the smaller park didn't seem to be as significant as a benefit when you have a larger gated park right next to it, so that is not being proposed right now and is off the table. He said the larger park would be for resident use and their contribution would be towards the adjacent public park, or whatever is decided. He said they are still talking to the parks department about getting the biggest bang for their buck and possibly improving the existing public park as an offered public benefit. Member Smith said he understands that this is a small site, and it is hard to squeeze a public benefit into the site, but this may be an opportunity to think outside of the box and do something like solar panels on the roofs, inground heat exchangers for heat pumps to be more energy efficient and have less of an environmental impact. He said it is a public benefit to the residents, but it is also a public benefit because of proof of concept, and it helps save energy and that is good for everyone. He said he would like to know if it will have broadband access and if it will be fiber wired. Mr. Bush said he is not sure at this time; it will likely be whatever Comcast offers as an upfront purchase. Member Smith said something like that could also be considered public benefit.

Member Gurmurthy said that the original plan was for 6 homes and now they are doing 10. She said that some of the concerns at the Planning Commission meeting were the characteristics of the homes, but once she saw photos, she was pleasantly surprised. She said she would like to see the results of the wetland investigation to see how it will be

preserved. She said that is something she will be looking for. She said that preservation will help with the invasive species treatment. She said that the proposed Lot #1 is adjacent to the wetland, so it will be important to properly delineate how to properly preserve the wetland. She said that the applicant is proposing a 20-foot perimeter of landscape buffers from the lots, but only a few areas of the buffer show that landscape is being provided. She said she'd like to see additional trees, as many as possible, protecting the adjacent residents as well as providing green space and a crosswalk. She said that there were notes about a crosswalk from the Planning commission and assumes there is going to be a study done and if not, she would request a study on that to understand all of the safety concerns. She said that there are suggestions to put a crosswalk here and there, but a study would be the most informative about the safety and benefits of the placement. She said the paved trail is nice for a nature trail, and that it is great. She said that she read a resident comment about storm water runoff and asked the applicant how they are prepared to handle that. Mr. Bush said it is still very early and all of that will be flushed out with engineering, but as of right now they are anticipating an underground detention chamber that is represented in the plans. He said they will be capturing the way the contours work, and they do run off site right now. He said they will be capturing most of the lots in the site, they have rear yard storm detention areas to detain it, slow it down, and send it out to 8 Mile Road where there is a natural outlet for everything. Member Gorumurthy said that the last thing she will be looking for is energy efficiency and environmental impacts, to do whatever they can as far as LEAD. She said these are all custom homes, so it is a great opportunity to do so.

Member Heintz said he wants to demonstrate his support for what was already said by previous councilmembers. He said he is continuing to learn the scope and the potential impact for any PRO. He said it is a small space to be doing anything in that area and understands the difficulty of offering a public benefit there. He said given its proximity to Maybury State Park, it highlights the proximity to beautiful landscape features and that is all the more reason to try and be a leader in any sort of green sustainability facilities and homes. He said to try and do their best to really demonstrate a public benefit and be a leader for others to follow.

Member Thomas said Member Smith mentioned something about the proposed sidewalk to Maybury Park, and she agrees with his comments that it may not be something they would want to pursue. She said that she does love the idea of the applicant contributing to ITC, and that money spent is a public benefit. She said if there was a decision to make a proposal to switch around the public benefit and push more towards ITC. She said it is the applicant's choices and ideas, but she loves the idea of supporting ITC. She said they have a grand vision of ITC, so any public benefit money going into it would be great for the entire community. Mr. Bush said that it would be acceptable to contribute to ITC park improvements. He said for clarification, the crosswalk location that is shown on the provided plan, is not their proposed crosswalk, it is what is already existing, and they have no plan on scrubbing that and moving it, as it was painted relatively recently. He said he does not have the details, and it may have been put in even with Ballentyne, but it is already existing. He said in order to connect it to Maybury Park; they would have to move it.

Member Staudt said that about 10 or 12 years ago he proposed a sidewalk on 8 Mile Road. He said that there was nothing there and a lot of empty fields. He said this proposal is kind of the last of the developments in that area. He said he would like to thank the gentlemen for bringing forward a proposal, this is single family homes. He said they evaluate a lot of projects. He said this is the kind of thing that is simple for himself to support. He said a public benefit in a 10 home; very small area is extraordinarily difficult and takes some imagination to come up with something that is truly worthwhile. He said he hopes that the City staff will hold pressure on the applicant to come up with something that is a little but more robust than what they are currently offering. He said the project is definitely something that the City of Novi could be proud to represent and thanked the applicant for bringing it forward.

Mayor Pro Tem Casey asked if the crosswalk, as it exists today, was a part of Ballentyne RUD requirement. Humna Anjum, Engineering, said that the crosswalk was a part of the site plan for Ballentyne. She said that relocating it was briefly discussed with staff, and the issue was the turning movements for going in and out of Maybury State Park. She said of course, engineering prefers to keep it where it is to avoid issues with pedestrians crossing and vehicles turning in and out of the developments and the park. Member Casey said that having a crosswalk on 8 Mile Road frightens her a bit because that area is a 50-mph speed limit with no pedestrian barriers and no safety island for pedestrians in the middle. She said, not for this project but in general, the Council may need to consider making a safety improvement there. She said that she will tend to echo the majority of the previously made comments, and there is some pretty significant consensus within the council. She said she tends to pay a lot of attention to how much screening is between a new development coming in and the developments around it. She said that the half acre lots are significant from a setback perspective, but she also suspects that the back yards will be large as well. She said to pay attention to how much screening the new development will have from the existing houses, so they will not be surprised when they come back for a review. She said this would be a great addition to this part of the town. She said this was thoughtfully designed about how it would fit with the surroundings and will be a perfect place for what they are planning. She said she would agree that they are not sure that a pathway through a state park is something they will be able to accomplish. She said that with the closeness of ITC Park, there is a strong recommendation that if the applicant chose to increase their contribution, that would be a good opportunity as well. She said she looks forward to seeing this again.

#### **4. Recommendation to adopt amended Novi Retirement Health Care Fund (OPEB) Investment Policy**

City Manager Cardenas said this recommendation comes from the Finance Administration Committee which came up after engaging with a third-party reviewer, AON, to look at the City's OPEB investment policy and provide recommendations to improve long term sustainability and reduce portfolio volatility. He said that Tina Glenn, Assistant City Treasurer, was present to provide an overview and was supported by Ken

from Morgan Stanley who is the City's investment advisor to answer any questions they may have.

Tina Glenn, Assistant City Treasurer, said in July of 2024, the Finance and Administration Committee recommended hiring a third-party consultant to review the fund's investment strategy and make recommendations. She said the findings were presented to the committee in April and it was at that time that it was decided that they would like to move forward with those recommendations. She said the first step in that process is to amend the investment policy. She said the changes primarily include some cosmetic changes to the verbiage, but there are two main changes. The first is in section G, which refers to the objectives. She said now that the investments are fully funded, they are moving away from an emphasis on long-term growth of principle and moving toward an asset preservation and risk reduction. She said the second main change is to update the asset allocation as it is currently a 50/50 mix between equities and fixed income. She said they are looking to incorporate the recommendations of the contract of the consultant and that would change the 50/50 mix to a 40/60 mix in reference to equities and fixed income. She said that Morgan Stanley is represented at the meeting, and they have been with the City since the inception of the fund in 2003.

Member Staudt said that the Finance Committee asked that this be brought forward several years ago, maybe a year and a half ago. He said that Mayor Fischer has a tremendous amount of experience with the topic. He said they got outstanding guidance from the consultant and sat through a presentation where they were all kind of glazed over, except for the Mayor. He said the recommendations, and every option was weighed out. He said they have fiduciary responsibility as Council Members to make sure that their assets are protected. He said he was pleased with the outcome and felt very confident that the information is good and protects not only the employees, but the City from potential spikes and downturns in the market.

**CM 25-05-68 Moved by Staudt, seconded by Casey: MOTION CARRIED: 7-0**

**Approval to adopt amended Novi Retirement Health Care Fund (OPEB) investment policy.**

**Roll call vote on CM 25-05-68**

**Yeas: Thomas, Fischer, Casey, Gurumurthy,  
Heintz, Smith. Staudt  
Nays: None**

**CONSENT AGENDA REMOVALS: None**

**AUDIENCE COMMENT: None**

**COMMITTEE REPORTS:**

1. Mobility Committee - Councilmember Smith